

## **Appendix C: Green River Ranch Visual Impact Analysis**

**This Page Intentionally Left Blank**

# GREEN RIVER RANCH

## ***VISUAL IMPACT ANALYSIS***

**GENERAL PLAN AMENDMENT NO. 2020-0002**

**SPECIFIC PLAN AMENDMENT NO. 2020-0006**

**TENTATIVE TRACT MAP NO. 37963**

**PRECISE PLAN NO. 2020-0004**

***LEAD AGENCY:***

CITY OF CORONA  
PLANNING DIVISION  
400 S. VICENTIA AVENUE  
CORONA, CA 92882

***PROJECT APPLICANT:***

PSIP WR GREEN RIVER LLC  
500 NEWPORT CENTER DRIVE, SUITE 630  
NEWPORT BEACH, CA 92660

***PREPARED BY:***



T&B PLANNING, INC.  
3200 EL CAMINO REAL, SUITE 100  
IRVINE, CA 92602

January 8, 2024

**TABLE OF CONTENTS**

<b><u>Section Name and Number</u></b>	<b><u>Page</u></b>
<b>1.0 Introduction .....</b>	<b>1</b>
<b>2.0 Existing Conditions .....</b>	<b>1</b>
2.1 Project Location.....	1
2.2 Site Topography .....	1
2.3 Existing Land Use and Development .....	1
2.4 Photographic Inventory.....	1
2.5 Scenic Highways .....	6
2.6 Applicable Land Use Policies .....	6
2.6.1 City of Corona General Plan .....	6
2.6.2 City of Corona Landscape Design Guidelines .....	11
2.6.3 Green River Ranch Specific Plan.....	11
<b>3.0 Project Description .....</b>	<b>12</b>
3.1 General Plan Amendment No. 2020-0002 (GPA2020-0002).....	12
3.2 Amendment No. 1 to the Green River Ranch Specific Plan (SPA2020-0006).....	12
3.3 Tentative Tract Map No. 37963 (TTM37963).....	14
3.4 Precise Plan No. 2020-0004 (PP2020-0004).....	14
3.4.1 PP2020-0004 Site Plan .....	14
3.4.2 PP2020-0004 Proposed Architecture .....	17
3.4.3 PP2020-0004 Landscaping Plan.....	22
3.4.4 PP2020-0004 Conceptual Grading Plan.....	26
<b>4.0 Evaluation Criteria .....</b>	<b>26</b>
<b>5.0 Visual Impact Analysis .....</b>	<b>26</b>
<b>6.0 Mitigation Measures.....</b>	<b>38</b>
<b>7.0 Conclusion .....</b>	<b>38</b>

**LIST OF FIGURES**

<b>Figure Name and Number</b>	<b>Page</b>
Figure 2-1 Regional Location Map.....	2
Figure 2-2 Vicinity Map.....	3
Figure 2-3 Site Photos.....	4
Figure 2-4 Scenic Corridors.....	7
Figure 3-1 SPA2020-0006 Proposed Land Use Plan .....	13
Figure 3-2 Tentative Tract Map No. 37963.....	15
Figure 3-3 PP2020-004 Conceptual Site Plan .....	16
Figure 3-4 Building 1 Conceptual Elevations (1 of 2).....	18
Figure 3-5 Building 1 Conceptual Elevations (2 of 2).....	19
Figure 3-6 Building 2 Conceptual Elevations .....	20
Figure 3-7 Building 3 Conceptual Elevations .....	21
Figure 3-8 Building 4 Conceptual Elevations .....	23
Figure 3-9 Building 5 Conceptual Elevations .....	24
Figure 3-10 Conceptual Landscape Plan.....	25
Figure 5-1 Conceptual Grading Plan .....	27
Figure 5-2 Conceptual Rendering – Aerial Perspective .....	29
Figure 5-3 Conceptual Rendering – Westerly Perspective .....	31
Figure 5-4 Street-Level Rendering – Green River Road at Fresno Road.....	33
Figure 5-5 Street-Level Rendering – Green River Road West of Private Street “A” .....	34
Figure 5-6 Street-Level Rendering – Green River Road East of Private Street “A” .....	35
Figure 5-7 Street-Level Rendering – Green River Road at Dominguez Ranch Road.....	36
Figure 7-1 Site Photometrics – Building 1 .....	39
Figure 7-2 Site Photometrics – Buildings 2, 3, 4 and 5 .....	40

## 1.0 Introduction

The purpose of this report is to provide an objective analysis of potential visual quality impacts that may result from implementation of the proposed Green River Ranch project (“Project”). The Project site comprises 160.00 acres and is located south of State Route 91, southwest of Dominguez Ranch Road, and southeast of Fresno Road within the City of Corona, California. The Project consists of applications for General Plan Amendment No. 2020-0002 (GPA2020-0002), Amendment No. 1 to the Green River Ranch Specific Plan (SPA2020-0006), Precise Plan No. 2020-0004 (PP2020-004), and Tentative Tract Map No. 39763 (TTM37963). Approval of these applications would allow for future development of the property with 5.5 acres of “General Commercial” uses, 49.31 acres of “Business Park Industrial” uses, up to 32 “Estate Residential” dwelling units on 20.39 acres, the remaining acreage will encompass 83.34 acres of “Open Space General” land uses.

## 2.0 Existing Conditions

### 2.1 Project Location

As shown on Figure 2-1, *Regional Location Map*, the Project site is located within the western portion of the City of Corona within the County of Riverside. The City of Corona is situated approximately 45 miles southeast of Los Angeles, and is located immediately south of the 91 freeway (SR-91) and west of the 71 freeway (SR-71) interchange. Regional access to the Project site is available from SR-91 and Green River Road. More specifically, and as depicted on Figure 2-2, *Vicinity Map*, the 160.00 -acre Project site is located south of State Route 91, southwest of Dominguez Ranch Road, and southeast of Fresno Road. Green River Road traverses the northern portions of the Project site in an east-west alignment.

### 2.2 Site Topography

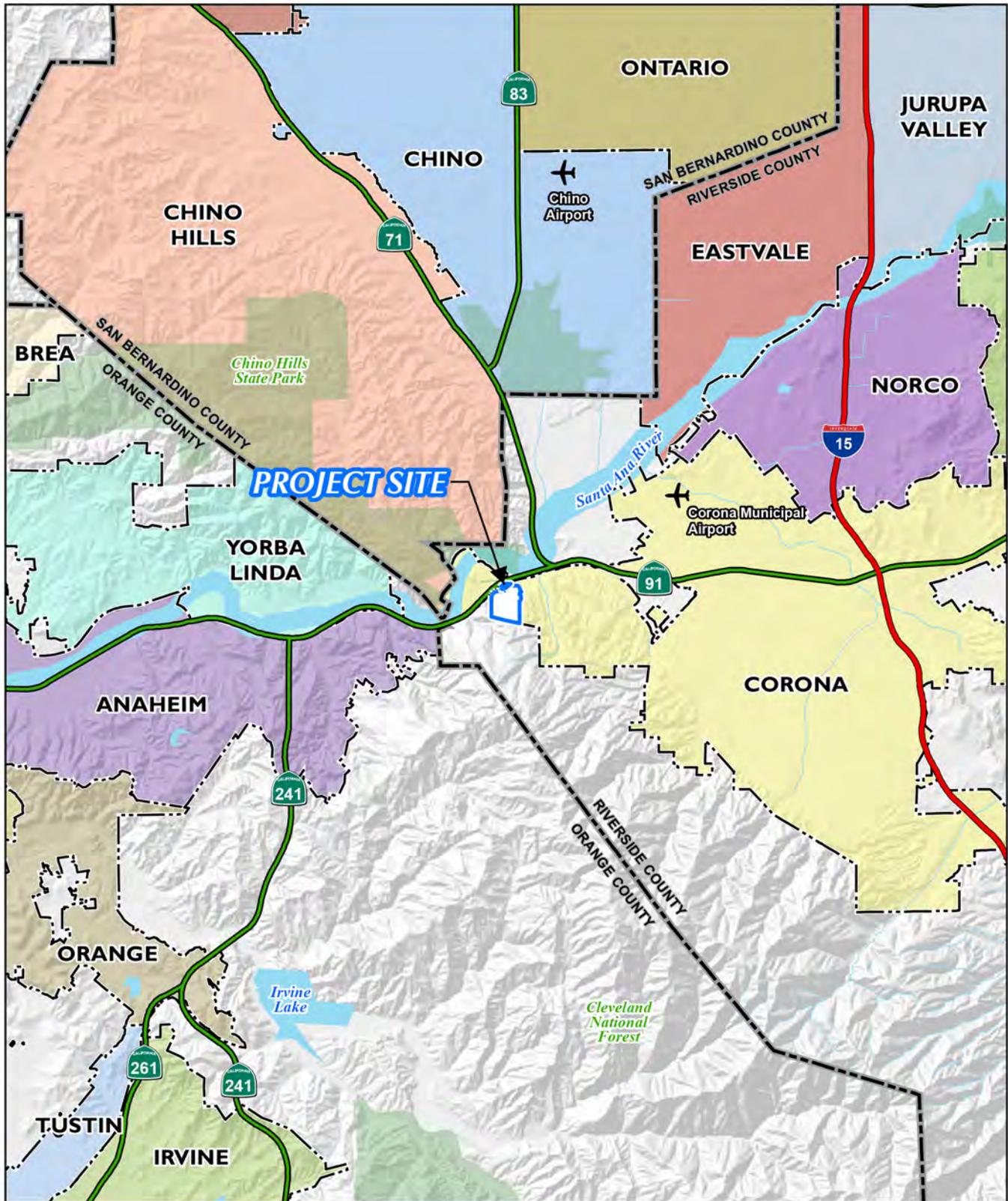
The 160.00-acre Project site is located at the base of several prominent landforms. The Project site exhibits relatively steep topography within the southern portions of the property, with the northern portions of the site comprising gently sloping lands. On-site elevations range from approximately 500 feet above mean sea level (amsl) in the northern portions of the site to approximately 1,108 feet amsl near the southwest corner of the property. Overall topographic relief is approximately 608 feet.

### 2.3 Existing Land Use and Development

Under existing conditions, the Project site is largely undeveloped, with exception of two existing single-family residential structures in the eastern portions of the site, while the central and northern portions of the property are currently being used as equestrian stables. The southern portions of the Project site are mostly covered with native vegetation, much of which consists of low-lying scrub.

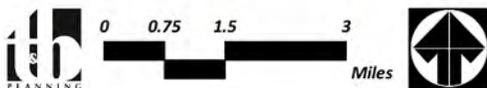
### 2.4 Photographic Inventory

A photographic inventory was prepared for the Project site in order to depict the existing visual characteristics of the Project site and surrounding areas. The existing conditions site photos are shown on Figure 2-3, *Site Photos*, and are described below.

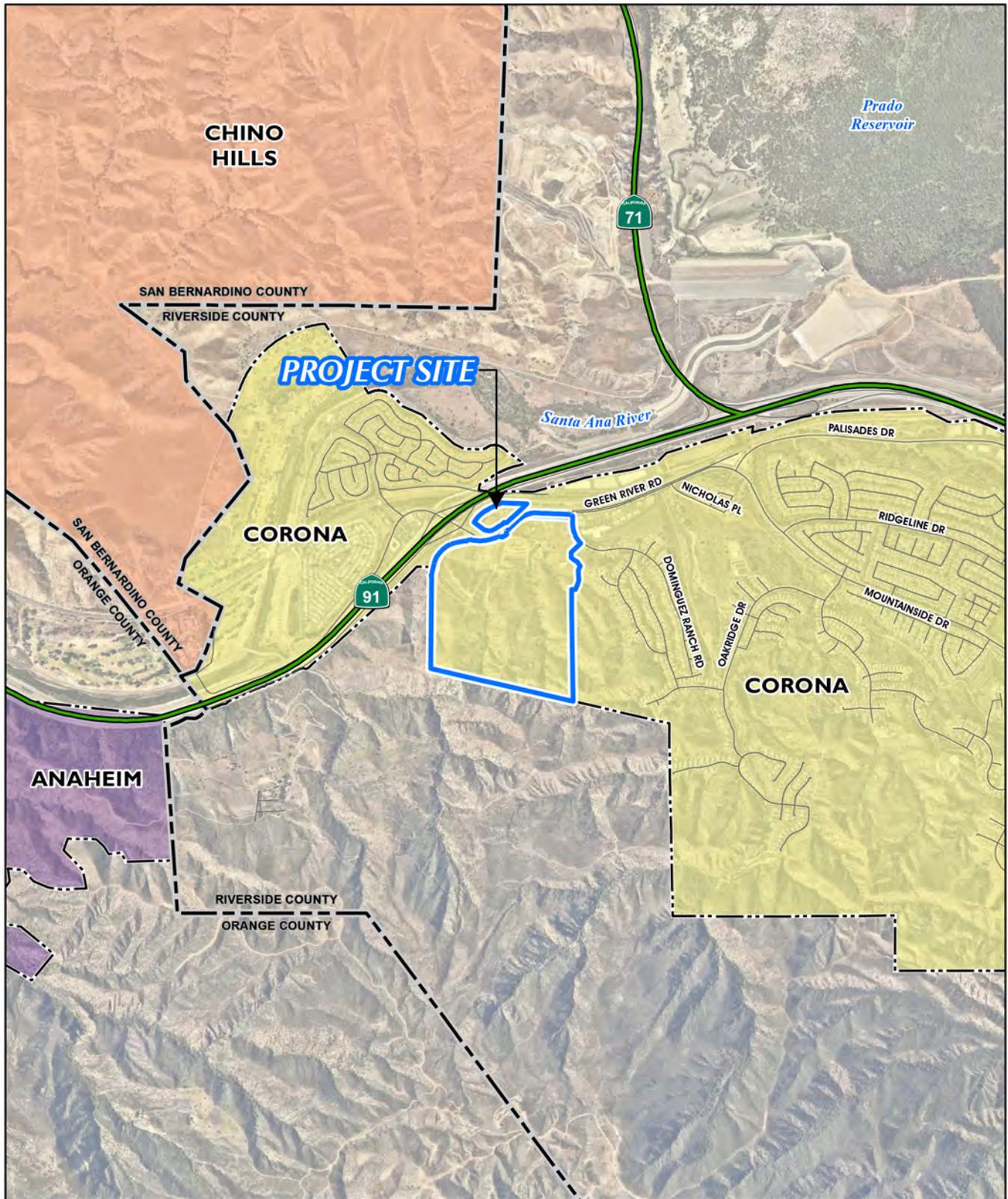


Source(s): ESRI, RCLMA (2020), OC (2019), SB County (2020)

Figure 2-1

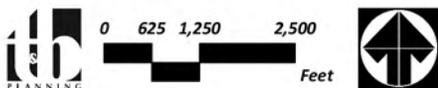


Regional Location Map



Source(s): ESRI, RCLMA (2020), OC (2019), SB County (2020)

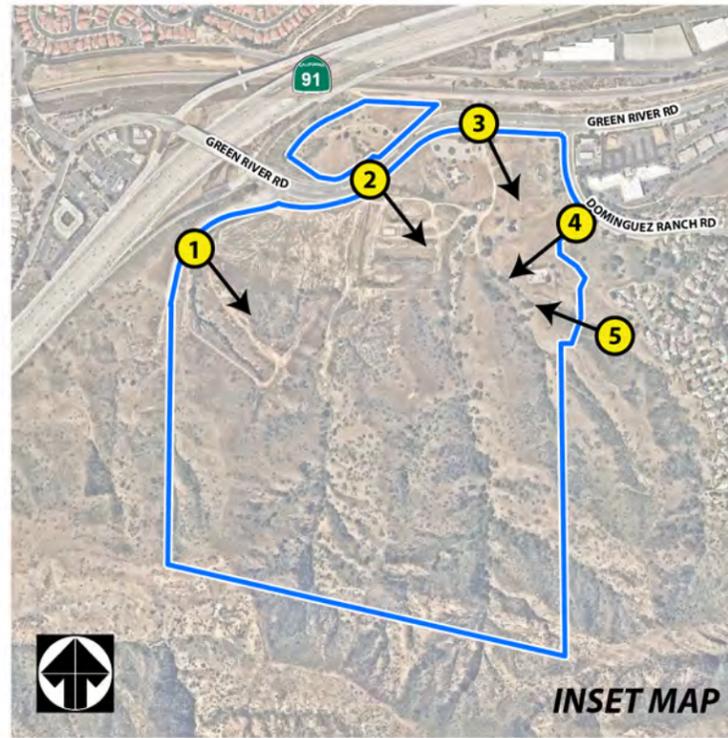
Figure 2-2



Vicinity Map



NORTHWESTERN PROPERTY LINE - Facing Southeast



EASTERN PROPERTY LINE - Facing West from Neighborhood



NORTHERN PROPERTY LINE - Facing Southeast



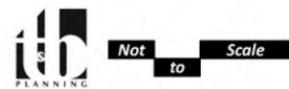
NORTHERN PROPERTY LINE - Facing Southeast



NORTHEASTERN PROPERTY LINE - Facing Southwest

Source(s): Bastien and Associates, Inc. (12-16-2019)

Figure 2-3



- Site Photo 1. Site Photo 1 was taken near the northwestern property line, facing southeast. As shown, the northern portions of the Project site currently consist of equestrian uses, with several horses visible in this photo. Tires and troughs can be seen throughout this portion of the site. The northern portions of the site consist of gently sloping terrain with sparse vegetation. The southern portions of the Project site also are visible in the distance, and appear from this location as steeply sloping terrain that contains a mixture of disturbed and natural vegetation.
- Site Photo 2. Site Photo 2 was taken along the Project site's frontage with Green River Road, looking south. As shown in this photo, Green River Road appears in the foreground as an improved roadway with ornamental landscaping in the median, and natural vegetation along the Project frontage. A concrete pad associated with a former commercial building is visible on the Project site in the near ground, along with several ornamental trees. In the right-hand portion of the photo in the near ground are relatively flat lands with disturbed natural low-lying vegetation. In the middle ground in the central portion of the photo, the two existing single-family residential structures are visible. In the distance, the steeply sloping terrain that characterizes the southern portions of the Project site is visible.
- Site Photo 3. Site Photo 3 was taken along the site's frontage with Green River Road, just west of Dominguez Ranch Road, facing southeast. In the foreground are several large trees and disturbed lands, portions of which consist of unvegetated dirt. The gently-sloping topography that characterizes the northern portions of the Project site are visible in the middle ground, along with an existing telephone pole that occurs near the site's eastern boundary. In the distance is steeply sloping terrain. Several existing single-family homes located off-site to the east of the Project site are visible atop a hill.
- Site Photo 4: Site Photo 4 was collected along the Project site's frontage with Dominguez Ranch Road, facing southwest. Ornamental landscaping is visible in the near ground, beyond which is disturbed grassland that is used for horse grazing. Several trees are visible in the distance in the middle portion of the photo. An existing single-family residential structure is visible in the distance in the right portion of the photo. In the left portion of the photo is natural vegetation (grassland) located off site. In the distance, the steeply sloping terrain that characterizes the southern portions of the Project site is visible.
- Site Photo 5: Site Photo 5 was taken from an existing single-family neighborhood located east of the Project site, and shows views to the west from this neighborhood. In the left portion of the photo, the steeply sloping hills in the south-central portions of the Project site are visible, and contain natural low-lying vegetation. The northern portions of the Project site are visible in the distance in the right portion of the photo, and appear from this location to consist of gently sloping topography that is largely disturbed and contains little vegetation. Several telephone poles are visible, along with several pads that appear to have been graded. In the distance is the SR 91 freeway, beyond which an existing commercial retail center is visible. Several large cell phone towers are visible in the distance from this location. In the far distance are several large prominent natural hillforms associated with the Chino Hills State Park.

## 2.5 Scenic Highways

As shown on Figure 2-4, *Scenic Corridors*, the City of Corona General Plan identifies roadways within the City that are designated or eligible for designation as scenic corridors. Within the Project area, Palisades Drive and the segment of Green River Road between Palisades Drive and SR 91 are classified as “City Designated” scenic corridors. SR 91 and State Route 71 (SR 71) are identified as “State Eligible” scenic highways.

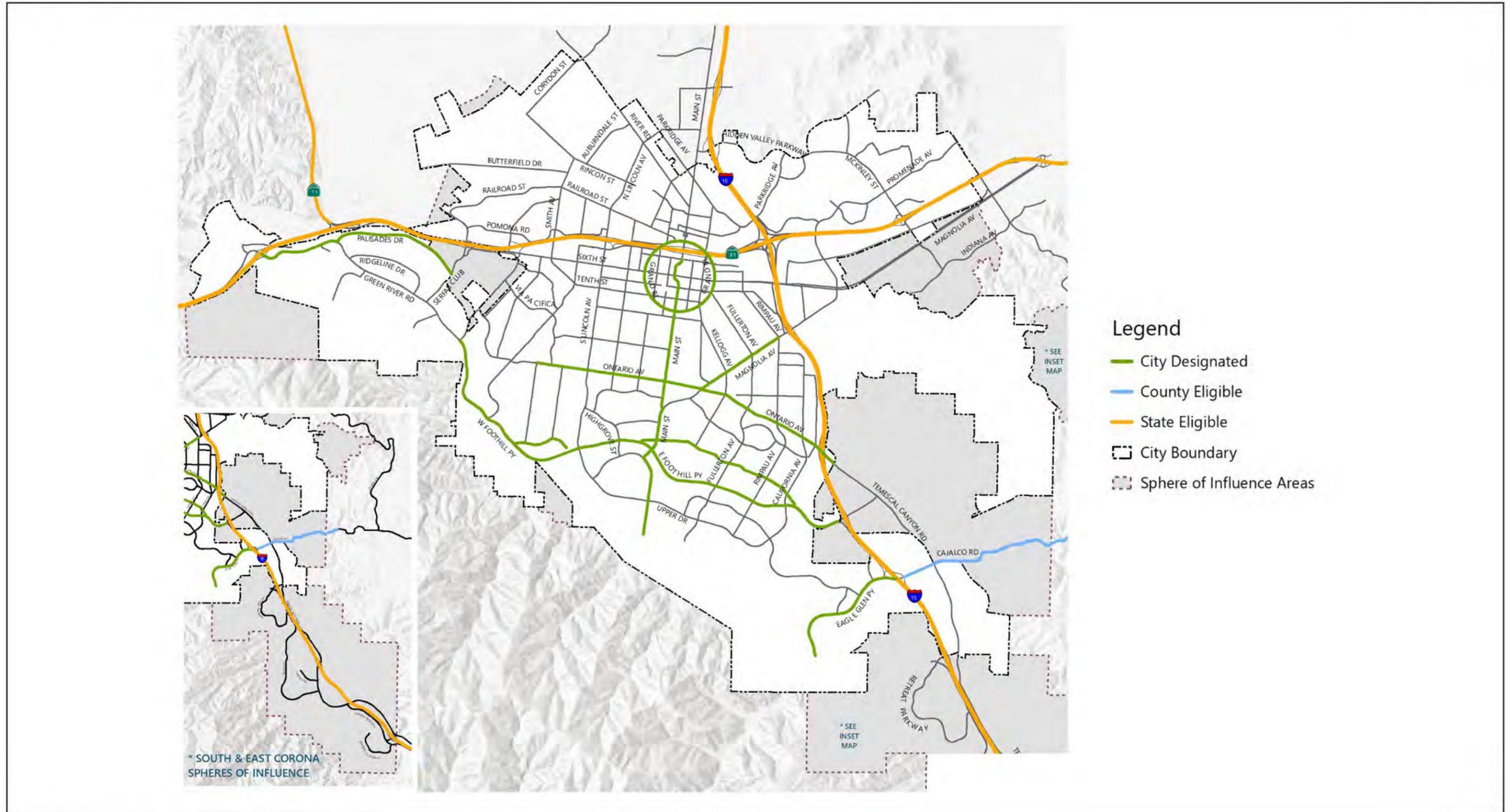
## 2.6 Applicable Land Use Policies

### 2.6.1 City of Corona General Plan

The City of Corona General Plan contains a number of goals and policies related to visual quality within the Community Design Element. Specifically, the Community Design Element contains goals and policies related to design context; street landscaping; City entries and monumentation; community signage and wayfinding; trails and greenways; district, nodes, and corridors; and visual resources. A listing of goals and policies related to the Community Design Element and that are applicable to the proposed Project is provided below. Consistency is addressed later in this document and in Appendix A of the proposed Specific Plan Amendment document.

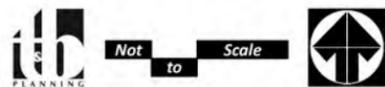
**Goal CD-1:** Public street landscapes that unify the City of Corona and contribute to the unique identity of its neighborhoods, districts, and public places.

- Policy CD-1.1** Require the planting and maintenance of street trees as part of development applications with consideration for the following principles:
- Physical constraints of the public street right-of-way, maintenance needs, and safety.
  - Importance of shade, beauty, and variety of species that encourage pedestrian activity.
  - Reflects the context of the surrounding residential, commercial, office, or industrial land uses.
- Policy CD-1.3** Maintain a street landscape master plan that identifies species to be used along public streets throughout the City. A palette of trees and landscaping should be designed to allow for:
- Differentiation within key districts in the City, including Downtown, Sixth Street, North Main, and other locations.
  - Differentiation at the street’s key nodal intersections, entryways, and public places.
  - Sustainable landscaping practices including but not limited to drought-resistant species.
- Policy CD-1.5** Require developers of residential subdivisions to submit a landscape plan that defines a program of trees and plantings that uniquely identify streets, principal entries and intersections, and activity centers such as parks and community facilities.



Source(s): Corona General Plan (2020)

Figure 2-4



**Goal CD-2:** Entries that are well defined by signage, landscaping, lighting, and other visual landmarks that provide a clear sense of arrival into and identity for the City of Corona.

**Policy CD-2.1** Implement improvements at key entries into Corona from the SR-91 and I-15 corridors that provide a distinct sense of arrival and identity. These may include well-designed signage, landscape, lighting, public art, monuments, fountains, structures, and other elements that serve as visual landmarks. Common elements (graphics, signage, etc.) should be used to visually distinguish the location as a primary City entry.

Potential locations include the SR-91 interchanges at McKinley Street, Main Street, Grand Boulevard, Lincoln Avenue, Sixth Street, Serfas Club Drive-Auto Center Drive, and Green River Road, and the I-15 interchanges at Magnolia Avenue, Ontario Avenue, and Cajalco Road.

**Policy CD-2.2** Coordinate the design of entry improvements with adjoining commercial and industrial property owners, where appropriate. Encourage the owners to incorporate landscape, signage, and architectural design elements in their projects that contribute to and complement the sense of entry from the freeways.

**Policy CD-2.3** Continue to promote the establishment of entry monumentation to identify and convey a distinct sense of arrival to designated communities, districts, and neighborhoods in Corona.

**Goal CD-3:** Well designed, high quality, and distinctive public and private signage that identifies key City districts, public facilities, buildings, and facilitates wayfinding.

**Policy CD-3.3** Work with private developers and property owners to develop on-site project signage for identification, traffic direction and wayfinding, and parking that complements the City's design program, where appropriate.

**Policy CD-3.4** Require that signage on private buildings be designed to exhibit a high quality of interest and visual appeal; be integrated into and reflect the building's architectural design character; and be sized to not overwhelm its scale and mass.

**Goal CD-4:** A network of trails and greenways that interconnect Corona's parklands, open spaces, and drainages that provide hiking and bicycle opportunities and access into surrounding open spaces and natural areas.

**Policy CD-4.2** New development adjoining open spaces, washes or have the ability to provide pedestrian connections to off-site trails or pathways should be designed to ensure landscape transitions and compatibility with these resources. Such improvements should be designed to provide adequate flood protection for adjoining properties.

**Goal CD-5:** A city of well-designed residential neighborhoods, commercial districts and corridors, industrial districts, and civic places that are uniquely identifiable in their building form, public places, and landscapes, contributing to a high quality of life for residents and positive image for visitors to the City.

**Policy CD-5.8** Require commercial and office centers to enhance their aesthetic quality, image, and “fit” with adjoining land uses. Elements may encompass site and entry identification by signage, landscape, or lighting; extensive on-site landscape; public art; improvements of abutting public streetscapes; and other amenities.

**Policy CD-5.9** Require that the renovation of existing buildings and new construction within freeway-oriented and community-oriented commercial centers exhibit a high and distinctive level of architectural character and site design character by adherence to policies in the land use and community design elements and established design guidelines.

**Policy CD-5.10** Require industrial projects to enhance their aesthetic quality, image, and “fit” with adjoining land uses. Elements may encompass site and entry identification by signage, landscape, or lighting; extensive on-site landscape; public art; improvements of abutting public streetscapes; and other amenities.

**Policy CD-5.11** Require that the renovation of existing buildings and new construction within industrial and business parks exhibit a high level of architectural and site design character by adherence to policies in the land use and community design elements, and established design guidelines.

**Goal CD-6:** Develop and implement land use controls that preserve significant visual resources from potential loss or disruption.

**Policy CD-6.1** Ensure unobstructed view corridors or viewsheds of the San Bernardino, Santa Ana, and San Gabriel Mountains, the Chino and La Sierra Hills, and other significant natural features from public spaces such as parks, termination of streets and community trails, community centers, and school properties, where feasible, as part of the design of development projects.

**Policy CD-6.2** Require that project applicants identify and map all slopes greater than 15 percent on parcels within the City’s hillside areas, referred to as the “Hillside Management District,” in increments of 5 percent. Lands within this District shall be subject to administrative review to ensure that development is located and designed to reflect its distinct environmental and topographic characteristics consistent with the policies of this plan, under the provisions of a Hillside Development Ordinance.

**Policy CD-6.3** Require that development in hillside areas with greater than 25 percent slope be clustered on the most gently sloping portions of the site, to the extent feasible,

according to the following density limitations of the underlying land use plan designations.

Maximum Percentage of Site to be Graded	Maximum Percent of Allowable Density
40-44.9%	100%
45-49.9%	90%
50-54.9%	80%
55-59.9%	70%
60-64.9%	60%
65-69.9%	50%
70-74.9%	40%
75-79.9%	30%
80-84.9%	20%
85+ %	10%

**Policy CD-6.4**

Require that projects be designed and sited to maintain the natural topographic, physiographic, and aesthetic viewshed characteristics of those features, utilizing the following conditions:

- Minimize the area and height of cuts and fills to the extent technically achievable, ensuring that slope tops and bottoms are rounded and facilitate a smooth and seamless transition where natural and built slopes intersect.
- Configure development sites to mimic predevelopment natural topography by clustering sites and individual units and avoiding extensive fragmentation of steep slopes, “stair stepping” and varying terraces of structures, and/or other design practices.
- Minimize the size of flat development pads in site grading to that necessary to accommodate the building footprint, a reasonable amount of useable outdoor space, and structural and site stability.
- Encourage building architectural design styles, forms and shapes, materials, and building siting to complement rather than visually dominate their landscape setting.
- Minimize the height of retaining walls, and design with smooth flowing forms that follow topography and with material colors and textures that blend in with the surrounding landscape.
- Plant hillside and canyon slopes with natural species of drought-tolerant plants to soften the visual impact of land grading, retaining walls, structures, and roads and maintain (to the extent feasible) natural vegetation.
- Restore disrupted vegetation, wildlife habitat, natural water courses, drainage swales, and other important viewshed features. Vegetation should be arranged in informal masses to create a textured slope characteristic of natural chaparral mountain slope terrain. (Imp 2)

**Goal CD-7:** Maintain, establish, develop, and protect the City's highways and corridors for scenic purposes.

**Policy CD-7.2** Regulate new development, substantial rehabilitation, or renovation projects through provisions that require an analysis of impacts of development on the quality of the City's designated highways and corridors.

**Policy CD-7.3** Prohibit installation or expansion of poles, billboards, and other above-ground appurtenances from detracting from the views along the City's scenic highways and corridors; phase out uses that impair scenic views.

### 2.6.2 City of Corona Landscape Design Guidelines

The City of Corona has adopted Landscape Design Guidelines for residential, commercial, and industrial developments. The purpose of these guidelines is to "create pleasant and attractive properties throughout the City" through "proper landscaping" in a manner that "promotes a sense of community, creates a more pleasant living and working environment, and promotes water and resource conservation." The goals of the Landscape Design Guidelines include the following:

- Ensuring the highest level of resource conservation including water conservation, ground water recharge, and green waste reduction;
- Promoting the design, installation, and maintenance of water-efficient landscaping, water use management and water conservation through the use of water-efficient landscaping, functional and limited use of turf grass, and aggressive use of water conserving irrigation technology and management.
- Reducing the water demands from landscapes without a decline in the landscape quality or quantity in a manner that conserves regional water resources, by ensuring that landscape projects are not unduly water-needy and that irrigation systems are appropriately implemented to minimize water waste;
- Retaining flexibility and encouraging creativity through appropriate design;
- Complying with state guidelines by requiring that landscapes not exceed a maximum water demand of seventy percent (70%) of its reference evapotranspiration (ET<sub>o</sub>) or any lower percentage as may be required by state legislation;
- Eliminating water waste from overspray and/or runoff; and
- Establishing procedural and design requirements for water efficient landscaping that are at least as effective in conserving water as the Department of Water Resources Model Water Efficient Landscape Ordinance in compliance with California Government Code section 65591 et seq.

### 2.6.3 Green River Ranch Specific Plan

Proposed Amendment No. 1 to the Green River Ranch Specific Plan contains a number of development standards and design guidelines related to visual quality. Subsection 2.2 of the Specific Plan indicates that grading activities associated with future development would be required to be conducted in a manner that closely resembles the natural terrain, and requires slopes to be landscaped in "natural-looking" arrangement in conformance with the City of Corona Landscape Design Guidelines. Subsection 3.3 establishes the development standards within the Specific Plan area, and identifies requirements for building setbacks, heights, lot coverage, and other

requirements. Subsection 3.3 also includes supplemental standards intended to reduce adverse visual effects associated with future development of the Specific Plan area, and includes standards related to walls and fencing; signage; entry monumentation; parking; and off-street loading. Section 4.0 of the Specific Plan includes design guidelines related to site planning; landscape architecture; entry treatments; parking areas; wildland/urban interface treatments; walls and fences; lighting; and architectural character. Future development on site would be required to comply with all applicable development standards and design guidelines as established by proposed Amendment No. 1 to the Green River Ranch Specific Plan.

### **3.0 Project Description**

The Project as evaluated herein consists of applications for a General Plan Amendment (GPA2020-0002), Amendment No. 1 to the Green River Ranch Specific Plan (SPA2020-0006), Tentative Tract Map (TTM37963), and a Precise Plan (PP2020-0004). Each is discussed below.

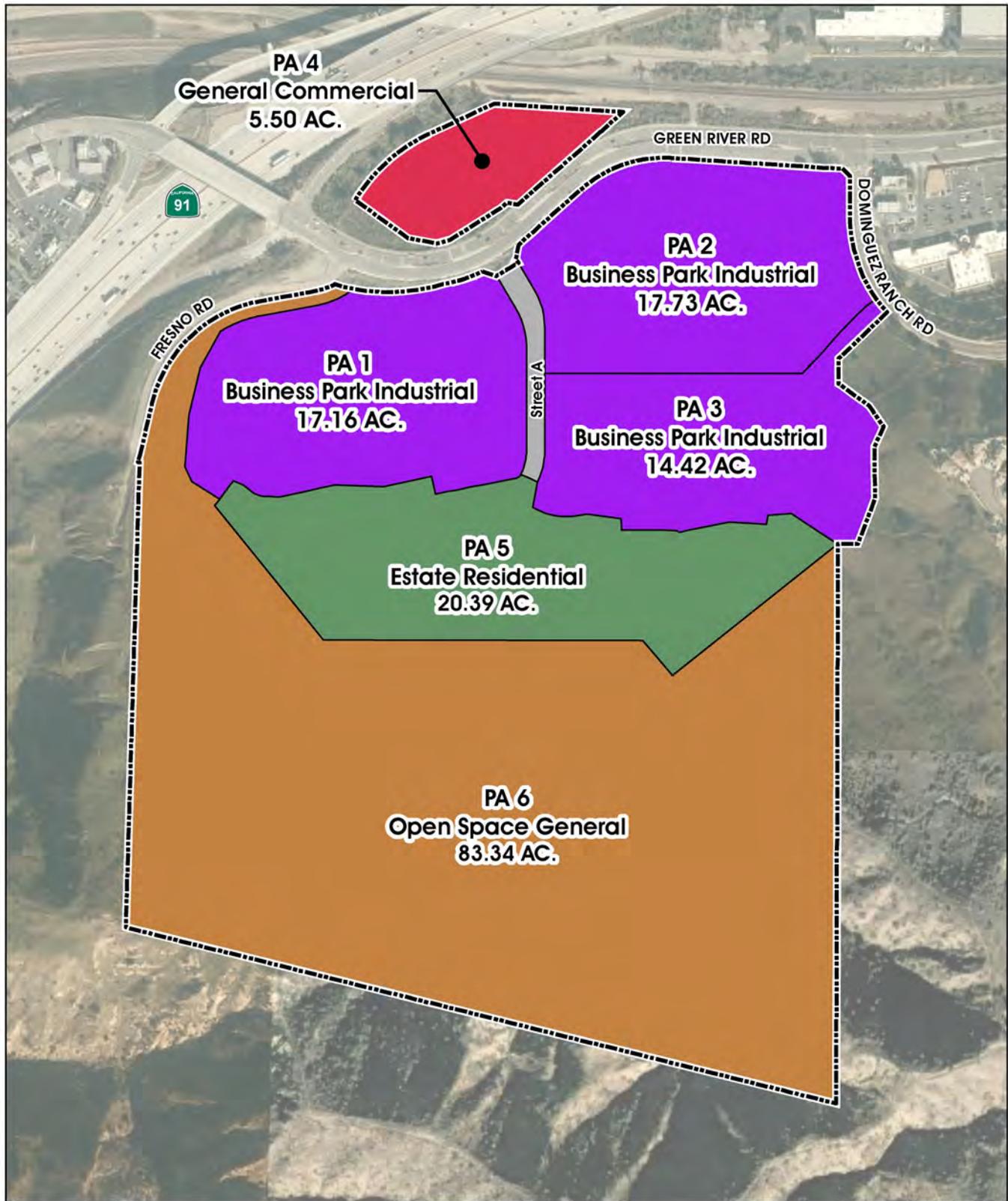
#### **3.1 General Plan Amendment No. 2020-0002 (GPA2020-0002)**

Under existing conditions, the City of Corona General Plan designates the 160.00-acre Project site for “Mixed Use: Industrial/Commercial (MU2),” “General Commercial (GC),” “Estate Residential (ER),” and “Open Space – General (OS-G)” land uses. As part of GPA2020-0002, the portion of the Project site located north of Green River Road would be redesignated from MU2 to GC on approximately 5.5 acres. To the south of Green River Road, areas currently designated for MU2, ER, and OS-G land uses would be reconfigured to instead provide for 49.31 acres of Business Park Industrial (BPI) land uses in the northern portions of the site, 20.39 acres of ER land uses south of the BPI land use designation, and 83.34 acres of Open Space would be dedicated in the southern portions of the site. Areas designated for OS-G would be redesignated for BPI land uses.

#### **3.2 Amendment No. 1 to the Green River Ranch Specific Plan (SPA2020-0006)**

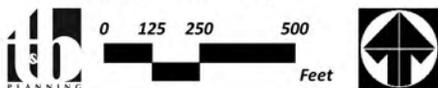
Under existing conditions, the Green River Ranch Specific Plan (GRRSP) designates 31.5 acres of the site for “Mixed Use,” 5.8 acres for “Hotel/Mixed Use Office,” 5.0 acres for “General Commercial” uses, and 98.2 acres for “Estate Residential” uses, with the remaining portions of the site designated for manufactured slopes and public streets. As shown on Figure 3-1, *SPA2020-0006 Proposed Land Use Plan*, the 5.5 acres of the Project site located north of Green River Road would be redesignated from “Mixed Use” to “General Commercial” as part of proposed Planning Area 4. Areas to the south of Green River Road would be redesignated from “Mixed Use,” “Hotel/Mixed Use Office,” and “General Commercial” to instead provide for “Business Park Industrial” land uses on 49.31 acres within proposed Planning Areas 1, 2, and 3 in the northern portions of the site, “Estate Residential” uses on 20.39 acres within proposed Planning Area 5, and 83.34 acres of “Open Space – General” in the southern portion of the site. Major roadways on site would be reconfigured to provide a single north-south oriented access road (Street A), which would connect to Green River Road in the north and would provide primary vehicular access to Planning Areas 1, 2, 3, and 5.

The “Business Park Industrial” land use designation is intended to accommodate single-tenant light industrial, warehouse, and incubator uses with supporting offices. Limited sales and services for industrial construction, and agricultural or similar type of uses also would be allowed. The “General Commercial” land use designation is intended to provide services for travelers and local residents, and would allow for service stations, restaurants (fast food, turnover, and high quality), and neighborhood retail. The “Estate Residential” land use designation is intended to provide for single-family detached residences on estate properties, and would allow for up to 32 dwelling units with a minimum lot size of 25,000 square feet. The “Open Space General” land use designation is intended for land conservation and preservation of the sites natural state.



Source(s): NearMap Aerial (2021), City of Corona (2020)  
Composite: KWC Engineers (08-19-2021)

Figure 3-1



In addition to identifying allowed land uses, SPA2020-0006 also identifies a conceptual grading plan and a conceptual infrastructure plan; provides development standards to govern future development on site; and includes Design Guidelines for site planning, architectural character, and landscape architecture to articulate the intended character of future development on site. SPA2020-006 also includes an implementation chapter that would govern development phasing, financing, maintenance responsibilities, and administration of the Specific Plan.

### 3.3 Tentative Tract Map No. 37963 (TTM37963)

As depicted on Figure 3-2, *Tentative Tract Map No. 37963*, Tentative Tract Map No. 37963 (TTM 37963) is proposed to subdivide the 154.50 acres of the Project site located south of Green River Road into nine (9) lots. Lot 1 would measure approximately 17.16 acres, and would encompass proposed Planning Area 1 of the Green River Ranch Specific Plan. Lots 2 and 3, measuring 8.69 acres and 9.04 acres, respectively, would establish lots for Buildings 2 and 3 within proposed Planning Area 2 of the Green River Ranch Specific Plan. Lots 4 and 5, measuring 6.13 acres and 8.29 acres, respectively, would establish lots for Buildings 4 and 5 within proposed Planning Area 3 of the Green River Ranch Specific Plan. Lot 6 would encompass approximately 83.34 acres, and would be provided for "Open Space – General" land uses within proposed Planning Area 6 of the Green River Ranch Specific Plan. Lot 7 would encompass approximately 20.39 acres, and would be provided for future development of "Estate Residential" land uses within proposed Planning Area 5 of the Green River Ranch Specific Plan. Proposed Lot "A" would encompass 1.23 acres and would consist of a private street providing access between Green River Road and the proposed "Estate Residential" uses in the south. Proposed Lot "B" would consist of 0.23 acres and would consist of proposed right-of-way for Green River Road.

### 3.4 Precise Plan No. 2020-0004 (PP2020-0004)

Precise Plan No. 2020-0004 (PP2020-004) is required to allow for the future construction of five (5) light industrial buildings within proposed Planning Areas 1, 2, and 3, consistent with the "Business Park Industrial" land use designation that would be applied to these portions of the Project site as part of SPA2020-0006. A description of the key elements of PP2020-004 is provided below.

#### 3.4.1 PP2020-0004 Site Plan

Figure 3-3, *PP2020-004 Conceptual Site Plan*, depicts the location and orientation of the proposed buildings, which are described below.

- Building 1 is proposed within proposed Planning Area 1, and would contain a total of 296,737 s.f. of building area (285,535 s.f. building footprint and 11,202 s.f. of mezzanine space). Truck trailer loading docks are proposed along the southern side of the building, with passenger vehicle parking areas occurring to the west, north, and east of the building, as well as to the south side of the truck trailer docking area. Access to Building 1 would be provided from a driveway along Street A.
- Buildings 2 and 3 are proposed within proposed Planning Area 2. Building 2 would contain a total of 131,695 s.f. of building area (125,588 s.f. building footprint and 6,107 s.f. of mezzanine space). Building 3 would contain a total of 130,980 s.f. of building area (124,693 s.f. building footprint and 6,287 s.f. of mezzanine space). As shown on Figure 3-2, truck trailer loading docks are proposed along the east side of Building 2 and along the west side of Building 3. Passenger vehicle parking areas are proposed to the

LEGAL DESCRIPTION:

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF CORONA IN THE COUNTY OF RIVERSIDE STATE OF CALIFORNIA AND IS DESCRIBED AS FOLLOWS:

PARCEL 1: THAT PORTION OF PARCEL 14 WITHIN TRACT 1777... AND ALLOTTED TO JOHN J. SCULLY IN THE COUNTY OF RIVERSIDE IN THE CASE OF CHASEL ET AL. VS. SCULLY ET AL. ACTION NO. 7829 OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA IN AND FOR THE COUNTY OF RIVERSIDE, A CERTIFIED COPY OF WHICH APPEARS OF RECORD IN BOOK 611, PAGE 432 OF DEEDS, RIVERSIDE COUNTY RECORDS, RECORDED AS FOLLOWS:

RESIDING AT STATION NO. 1 AT THE SOUTHWEST CORNER OF TRACT 1777 ON SAID MAP THENCE NORTH 13.8162 FEET TO STATION NO. 2 THENCE NORTH 42.2014 FEET WEST 137.46 FEET TO STATION NO. 3 THENCE SOUTH 13.8162 FEET TO STATION NO. 4 THENCE SOUTH 2.1474 FEET TO STATION NO. 5 THENCE NORTH 42.2014 FEET WEST 137.46 FEET TO THE POINT OF BEGINNING SAID PARCEL BEING A PORTION OF TRACT 1777 AS SHOWN ON SAID MAP.

EXCEPT ANY PORTION WITHIN THE LAND CONVEYED TO THE STATE OF CALIFORNIA FOR FREWAY PURPOSES BY DEED RECORDED FEBRUARY 04, 1989 AS INSTRUMENT NO. 10869 OF OFFICIAL RECORDS.

ALSO DESCRIBE THEREIN THAT PORTION AS DESCRIBED IN FINAL ORDER OF CONFIRMATION CASE NO. 10869-10870-018-018 RECORDED JANUARY 26, 2009 AS INSTRUMENT NO. 1009-009-018 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

ALSO EXCEPT ANY MOBILE HOMES ON SAID LAND.

APR 101-182-018-0

PARCEL 2:

THAT PORTION OF PARCEL 14 WITHIN TRACT 1777... AND ALLOTTED TO JOHN J. SCULLY IN THE COUNTY OF RIVERSIDE IN THE CASE OF CHASEL ET AL. VS. SCULLY ET AL. ACTION NO. 7829 OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA IN AND FOR THE COUNTY OF RIVERSIDE, A CERTIFIED COPY OF WHICH APPEARS OF RECORD IN BOOK 611, PAGE 432 OF DEEDS, RIVERSIDE COUNTY RECORDS, RECORDED AS FOLLOWS:

RESIDING AT STATION NO. 1 AT THE SOUTHWEST CORNER OF TRACT 1777 ON SAID MAP THENCE NORTH 13.8162 FEET TO THE SOUTHWEST CORNER OF THE COUNTY HIGHWAY AS SHOWN ON THE COUNTY OF RIVERSIDE FLOOD CONTROL DISTRICT ACTION NO. 31409 OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA IN AND FOR THE COUNTY OF RIVERSIDE, A CERTIFIED COPY OF FINAL ORDER OF CONFIRMATION BEING RECORDED JUNE 16, 1941 IN BOOK 516, PAGE 252 AND IN BOOK 516, PAGE 12, RESPECTIVELY OF OFFICIAL RECORDS, RIVERSIDE COUNTY RECORDS.

ALSO EXCEPT THEREIN THAT PORTION WITHIN TRACT 1777 CONVEYED BY ORANGE COUNTY FLOOD CONTROL DISTRICT IN ACTION NO. 31409 OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA IN AND FOR THE COUNTY OF RIVERSIDE, A CERTIFIED COPY OF FINAL ORDER OF CONFIRMATION BEING RECORDED JUNE 16, 1941 IN BOOK 516, PAGE 252 AND IN BOOK 516, PAGE 12, RESPECTIVELY OF OFFICIAL RECORDS, RIVERSIDE COUNTY RECORDS.

ALSO EXCEPT THEREIN ANY PORTION WITHIN THE LAND CONVEYED TO THE STATE OF CALIFORNIA FOR FREWAY PURPOSES BY DEED RECORDED FEBRUARY 04, 1989 AS INSTRUMENT NO. 10869 OF OFFICIAL RECORDS.

ALSO EXCEPT THEREIN THAT PORTION WITHIN THE FOLLOWING:

COMMENCING AT THE SOUTHWEST CORNER OF SAID COURSE 78 AS DESCRIBED IN THE DEED RECORDED FEBRUARY 04, 1989 AS INSTRUMENT NO. 10869 OF OFFICIAL RECORDS, THENCE ALONG SAID COURSE 78 NORTH 42.2014 FEET EAST 90.45 FEET, THENCE AT RIGHT ANGLES NORTH 41.3138 FEET WEST 10.00 FEET TO THE POINT OF BEGINNING, THENCE NORTH 41.3138 FEET WEST 27.82 FEET TO THE BEGINNING OF A DRAINAGE CURVE (CENTRAL ANGLE OF 79.5714 AN ARC DISTANCE OF 40.44 FEET, THENCE WESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 79.5714 AN ARC DISTANCE OF 40.44 FEET, THENCE ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 79.5714 AN ARC DISTANCE OF 40.44 FEET, THENCE COURSE 78 NORTH 42.2014 FEET WEST 10.00 FEET TO STATION NO. 1, THENCE SOUTH 13.8162 FEET TO STATION NO. 2, THENCE NORTH 42.2014 FEET WEST 137.46 FEET TO STATION NO. 3, THENCE SOUTH 13.8162 FEET TO STATION NO. 4, THENCE NORTH 42.2014 FEET WEST 137.46 FEET TO THE POINT OF BEGINNING, THENCE SOUTH 13.8162 FEET WEST 10.00 FEET TO SAID TRUE POINT OF BEGINNING.

ALSO EXCEPT THEREIN THAT PORTION AS DESCRIBED IN FINAL ORDER OF CONFIRMATION CASE NO. 10869-10870-018-018 RECORDED JANUARY 26, 2009 AS INSTRUMENT NO. 1009-009-018 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

ALSO EXCEPT ANY MOBILE HOMES ON SAID LAND.

APR 101-182-018-0 101-182-018-0

PARCEL 3:

THAT PORTION OF TRACT 1777 MARKED 5, 6 AND 7, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, IN AND FOR THE COUNTY OF RIVERSIDE, A CERTIFIED COPY OF WHICH APPEARS OF RECORD IN BOOK 611, PAGE 432 OF DEEDS, RIVERSIDE COUNTY RECORDS, RECORDED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF SAID TRACT 1777 MARKED 5, 6 AND 7, AS SHOWN ON SAID MAP, THENCE NORTH 13.8162 FEET TO STATION NO. 1, THENCE SOUTH 13.8162 FEET TO STATION NO. 2, THENCE NORTH 42.2014 FEET WEST 137.46 FEET TO STATION NO. 3, THENCE SOUTH 13.8162 FEET TO STATION NO. 4, THENCE NORTH 42.2014 FEET WEST 137.46 FEET TO THE POINT OF BEGINNING, SAID PARCEL BEING A PORTION OF TRACT 1777 AS SHOWN ON SAID MAP.

EXCEPT THEREIN THAT PORTION WITHIN TRACT 1777 CONVEYED BY THE STATE OF CALIFORNIA FOR A STATE HIGHWAY IN ACTION NO. 31409 OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA IN AND FOR THE COUNTY OF RIVERSIDE, A CERTIFIED COPY OF FINAL ORDER OF CONFIRMATION BEING RECORDED OCTOBER 03, 1941 IN BOOK 516, PAGE 252 AND IN BOOK 516, PAGE 12, RESPECTIVELY OF OFFICIAL RECORDS, RIVERSIDE COUNTY RECORDS.

ALSO EXCEPT THEREIN THAT PORTION WITHIN TRACT 1777 CONVEYED BY ORANGE COUNTY FLOOD CONTROL DISTRICT IN ACTION NO. 31409 OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA IN AND FOR THE COUNTY OF RIVERSIDE, A CERTIFIED COPY OF FINAL ORDER OF CONFIRMATION BEING RECORDED JUNE 16, 1941 IN BOOK 516, PAGE 252 AND IN BOOK 516, PAGE 12, RESPECTIVELY OF OFFICIAL RECORDS, RIVERSIDE COUNTY RECORDS.

ALSO EXCEPT THEREIN ANY PORTION WITHIN THE LAND CONVEYED TO THE STATE OF CALIFORNIA FOR FREWAY PURPOSES BY DEED RECORDED FEBRUARY 04, 1989 AS INSTRUMENT NO. 10869 OF OFFICIAL RECORDS.

ALSO EXCEPT THEREIN THAT PORTION WITHIN THE FOLLOWING:

COMMENCING AT THE SOUTHWEST CORNER OF SAID COURSE 78 AS DESCRIBED IN THE DEED RECORDED FEBRUARY 04, 1989 AS INSTRUMENT NO. 10869 OF OFFICIAL RECORDS, THENCE ALONG SAID COURSE 78 NORTH 42.2014 FEET EAST 90.45 FEET, THENCE AT RIGHT ANGLES NORTH 41.3138 FEET WEST 10.00 FEET TO THE POINT OF BEGINNING, THENCE NORTH 41.3138 FEET WEST 27.82 FEET TO THE BEGINNING OF A DRAINAGE CURVE (CENTRAL ANGLE OF 79.5714 AN ARC DISTANCE OF 40.44 FEET, THENCE WESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 79.5714 AN ARC DISTANCE OF 40.44 FEET, THENCE ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 79.5714 AN ARC DISTANCE OF 40.44 FEET, THENCE COURSE 78 NORTH 42.2014 FEET WEST 10.00 FEET TO STATION NO. 1, THENCE SOUTH 13.8162 FEET TO STATION NO. 2, THENCE NORTH 42.2014 FEET WEST 137.46 FEET TO STATION NO. 3, THENCE SOUTH 13.8162 FEET TO STATION NO. 4, THENCE NORTH 42.2014 FEET WEST 137.46 FEET TO THE POINT OF BEGINNING, THENCE SOUTH 13.8162 FEET WEST 10.00 FEET TO SAID TRUE POINT OF BEGINNING.

ALSO EXCEPT THEREIN THAT PORTION AS DESCRIBED IN FINAL ORDER OF CONFIRMATION CASE NO. 10869-10870-018-018 RECORDED JANUARY 26, 2009 AS INSTRUMENT NO. 1009-009-018 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

ALSO EXCEPT ANY MOBILE HOMES ON SAID LAND.

APR 101-182-018-0

GENERAL NOTES:

PREPARED FEBRUARY 2023  
1. TOTAL PROJECT GROSS AREA IS 154.80 ACRES.

2. ACREAGE SUMMARY TABLE with columns: ACRES, PERCENT, and rows for Industrial Net Area, Open Space, Future Estate Residential, and Green Space.

3. PROPOSED NO. OF LOTS: 7 LOTS

4. EXISTING GENERAL PLAN DESIGNATION: GENERAL COMMERCIAL MIXED USE 2, EXISTING RESIDENTIAL, AND OPEN SPACE-GENERAL

5. PROPOSED GENERAL PLAN DESIGNATION: MIXED LOT 2, EXISTING RESIDENTIAL, AND OPEN SPACE-GENERAL

6. EXISTING LAND USE: MIXED

7. PROPOSED LAND USE: INDUSTRIAL, RESIDENTIAL, AND OPEN SPACE

8. EXISTING ZONING: GENERAL COMMERCIAL MIXED USE 2, EXISTING RESIDENTIAL

9. PROPOSED ZONING: BUSINESS PARK INDUSTRIAL, EXISTING RESIDENTIAL, AND OPEN SPACE

10. ADJACENT LAND USE: MIXED

11. GENERAL PLAN - GENERAL COMMERCIAL MIXED USE 2 - MIXED

12. GENERAL PLAN - OPEN SPACE GENERAL 1ST CLASS - OPEN SPACE

13. GENERAL PLAN - OPEN SPACE GENERAL 2ND CLASS - OPEN SPACE

14. GENERAL PLAN - BUSINESS PARK INDUSTRIAL MIXED USE 2 - MIXED

15. EXISTING USE - OPEN SPACE

16. EXISTING USE - INDUSTRIAL

17. EXISTING USE - RESIDENTIAL

18. EXISTING USE - FUTURE ESTATE RESIDENTIAL

19. EXISTING USE - OPEN SPACE

20. EXISTING USE - OPEN SPACE

21. EXISTING USE - OPEN SPACE

22. EXISTING USE - OPEN SPACE

23. EXISTING USE - OPEN SPACE

24. EXISTING USE - OPEN SPACE

25. EXISTING USE - OPEN SPACE

26. EXISTING USE - OPEN SPACE

27. EXISTING USE - OPEN SPACE

28. EXISTING USE - OPEN SPACE

29. EXISTING USE - OPEN SPACE

30. EXISTING USE - OPEN SPACE



OWNER: PDP INC GREEN RIVER LLC  
DEVELOPER: PDP INC GREEN RIVER LLC  
ENGINEER: KWC ENGINEERS

Table with columns: LOT NUMBER, PROPOSED GROSS AREA, and LAND USE. Lists lots 1 through 7 and Lot B with their respective areas and uses.

UTILITY NOTES:  
WATER: CITY OF CORONA DEPARTMENT OF WATER AND POWER  
SEWER: CITY OF CORONA DEPARTMENT OF WATER AND POWER  
POWER: SOUTHERN CALIFORNIA Edison CO  
GAS: SOUTHERN CALIFORNIA GAS CO

BASIS OF BEARINGS:  
THE BASIS OF BEARINGS FOR THIS MAP ARE TAKEN FROM A LINE BETWEEN THE CITY OF CORONA GPS POINTS 1163 AND 1165, AND BEING 420.42317'. UNLESS NOTED OTHERWISE ALL DIMENSIONS SHOWN ARE GROUND DISTANCES TO CORNER AND CENTER, MEASURED IN THE HORIZONTAL PLANE BY A FACTOR OF 0.999999977.

BENCHMARK:  
BENCH MARK NO. 0-81 ELEVATION: 512.16 AND ORIGINAL WIDTH: 100.00 X 2.00" (WOOD) DIMENSIONS: 2.00" X 2.00" IN THE TOP OF WESTERN CORNER OF CORONAZ ROAD 21' SOUTHWEST OF THE E.C.A. IN THE SOUTHWEST CORNER OF THE INTERSECTION OF CORONAZ ROAD AND GREEN RIVER ROAD.

STATEMENT OF PREPARER:  
I HEREBY STATE THAT THIS MAP WAS PREPARED UNDER MY SUPERVISION AND THAT THE OWNER OF RECORD HAS KNOWLEDGE OF AND CONSENTS TO THE PLANS OF THIS MAP.  
THOMAS M. CARROLL, P.E., P.L.S. DATE

Source(s): KWC Engineers (03-08-2023)

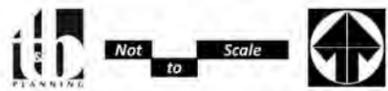


Figure 3-2

**Project Directory**

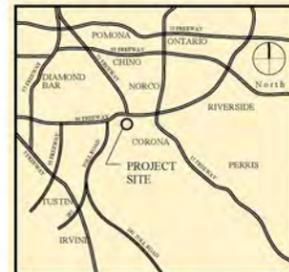
**DEVELOPER / APPLICANT:**  
 PSP WR Green River, LLC.  
 500 Newport Center Drive, Suite 630  
 Newport Beach, California 92660  
 Telephone: 949 720 3788  
 Contact: Gary Edwards  
 gedwards@westernrealco.com

**ARCHITECT:**  
 BASTIEN AND ASSOCIATES, INC.  
 15661 Red Hill Avenue, Suite 150  
 Tustin, California 92780  
 Telephone: 714 617 8600  
 Contact: Michael Visetto  
 mvisetto@bastienarchitects.com

**CIVIL:**  
 KWC ENGINEERS  
 1880 Compton Ave.  
 Corona, CA 92881-3370  
 Telephone: 951 734 2130  
 Contact: Mike C. Taing  
 mike.taing@KWCengineers.com

**LANDSCAPE:**  
 EMERALD DESIGN  
 305 N. Harbor Blvd., Suite 222  
 Fullerton, CA 92832  
 Telephone: 714 680 0417  
 Contact: Eric Freeman  
 eric@emeraldadesign.com

**Vicinity Map (N.T.S.)**



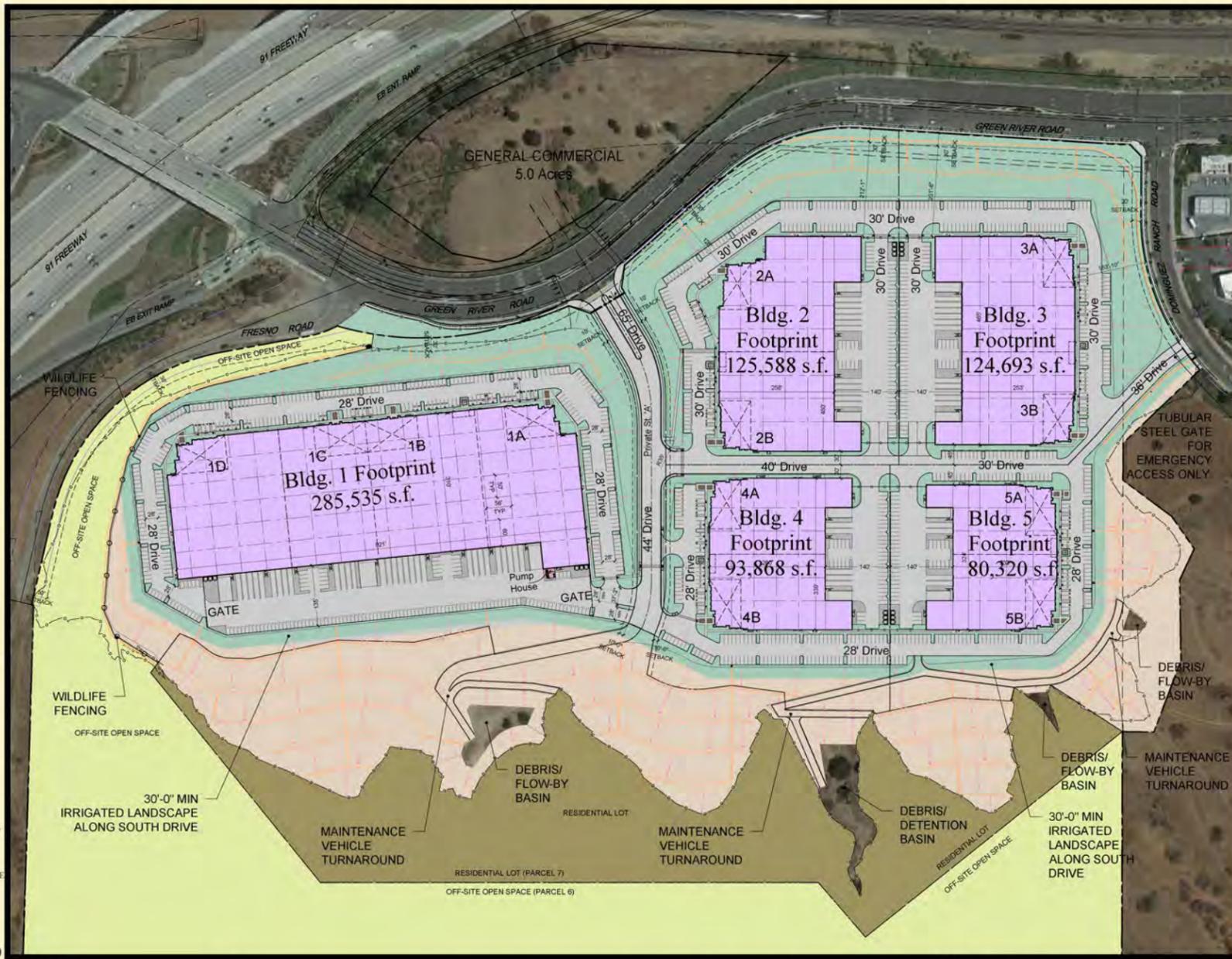
**Legend**

- Property Boundary Line
  - Building Setback Line
  - Lot Easement Line
  - Extent of Mfr. Slope
  - Potential Interior Wall Location
  - Decorative Fencing
  - Wildlife Fencing at Open Space
- 
- Solid Dark Hatch Represents Landscape Area
  - Diagonal Hatch Represents Painted Striping
  - Solid Light Hatch Represents On-Site Hardscape
  - Dark Hatch Represents Enhanced Hardscape
  - Solid Hatch Represents Hillside Revegetation
  - Solid Hatch Represents OFF-SITE OPEN SPACE
  - Solid Hatch Represents Residential Lot

**Planning Information**

(Refer to Civil for additional information)

Specific Plan: Green River Ranch Specific Plan SP00-001  
 Specific Plan Zone: Business Park Industrial  
 Bldg. Setbacks:  
 Front Building Setback:  
 Green River Road: 30 ft  
 Other Public Streets (Dominguez Ranch Rd & Fresno Rd): 20 ft  
 Private Streets (Street A): 10 ft  
 Rear Building Setback: No minimum  
 Interior Building Setback: No minimum  
 Street Side Building Setback: 15 ft



**Site Plan Summary**

Gross Site Area	(±) 49.31 Acres	± 2,148,077.48 s.f.
Excludes Parcels 6 & 7 (Refer to Civil Plans)		
Total Building Area	710,004 s.f.	746,167 s.f.
Ground Floor	710,004 s.f.	
Total Mezzanine Total	36,163 s.f.	
F.A.R. (Based on Gross Site Area)		34.73%
Landscape Area	± 647,754 s.f.	
Native Buffer / Fuel Mod Area	± 604,665 s.f.	

**BUILDING SUMMARY**

BUILDING	(Parcel)	F.A.R.	(Gross Parcel Area)	(Gross Parcel Area)
BUILDING 1	(Parcel 1)	.39	17.16 Acres	747,252.57 s.f.
Building Area				296,737 s.f.
Building Footprint				285,535 s.f.
Mezzanine				11,202 s.f.
(ESFR Pump House, (456 s.f.), not included in s.f.)				
BUILDING 2	(Parcel 2)	.34	8.69 Acres	378,285.77 s.f.
Building Area				131,695 s.f.
Building Footprint				125,588 s.f.
Mezzanine				6,107 s.f.
BUILDING 3	(Parcel 3)	.33	9.04 Acres	393,929.35 s.f.
Building Area				130,980 s.f.
Building Footprint				124,693 s.f.
Mezzanine				6,287 s.f.
BUILDING 4	(Parcel 4)	.37	6.13 Acres	267,132.84 s.f.
Building Area				100,155 s.f.
Building Footprint				93,868 s.f.
Mezzanine				6,287 s.f.
BUILDING 5	(Parcel 5)	.24	8.29 Acres	361,203.95 s.f.
Building Area				86,600 s.f.
Building Footprint				80,320 s.f.
Mezzanine				6,280 s.f.

**Parking Summary**

BUILDING	(Total s.f.)	Parking Provided	Parking Required
BUILDING 1	(296,737 s.f.)	461 Stalls	460 Stalls
Office (11%)	30,000 s.f. 1:250	120 Stalls	
Manufacturing (24%)	73,000 s.f. 1:500	146 Stalls	
Warehouse (65%)	193,737 s.f. 1:1,000	194 Stalls	
BUILDING 2	(131,695 s.f.)	195 Stalls	195 Stalls
Office (9%)	12,000 s.f. 1:250	48 Stalls	
Manufacturing (21%)	27,000 s.f. 1:500	54 Stalls	
Warehouse (70%)	92,695 s.f. 1:1,000	93 Stalls	
BUILDING 3	(130,980 s.f.)	220 Stalls	219 Stalls
Office (9%)	12,000 s.f. 1:250	48 Stalls	
Manufacturing (40%)	52,180 s.f. 1:500	104 Stalls	
Warehouse (51%)	66,800 s.f. 1:1,000	67 Stalls	
BUILDING 4	(100,155 s.f.)	174 Stalls	174 Stalls
Office (12%)	12,000 s.f. 1:250	48 Stalls	
Manufacturing (37%)	37,000 s.f. 1:500	74 Stalls	
Warehouse (51%)	51,155 s.f. 1:1,000	52 Stalls	
BUILDING 5	(86,600 s.f.)	160 Stalls	153 Stalls
Office (14%)	12,000 s.f. 1:250	48 Stalls	
Manufacturing (34%)	30,000 s.f. 1:500	60 Stalls	
Warehouse (52%)	44,600 s.f. 1:1,000	45 Stalls	

**Scope of Work**

- Five New Concrete Tilt Industrial Buildings.
- All new Drives Shall be Concrete Paving.
- All new Walks Shall be Concrete with Medium Broom Finish.
- Public Improvements as directed by City Staff.
- All new Landscaping Per City Requirements.

**General Notes**

1. Site Plan Shall Meet All Engineering and NPDES Requirements.
2. All Lighting Shall Conform with the Municipal Standards and Green River Ranch Specific Plan.
3. All Signage Shall Conform with the Municipal Standards and Green River Ranch Specific Plan.
4. All Hardscape Shown on Plan will be Installed as Concrete Paving.
5. All Parking Spaces are shown as Standard Size Stalls - No Compact Spaces are Anticipated.
6. All Mechanical Equipment and Screening Shall Conform with the Municipal Standards and Green River Ranch Specific Plan.
7. All Public Improvements Shall Conform with the Municipal Standards.

**Assessor Parcel Numbers (APN)**

101-180-014-8, 101-180-037-9, 101-180-038-0, 101-180-015-9

PA-5 NOTE:  
 Prior to proceeding forward with a Precise Plan for development in Planning Area 5 (PA-5), address two points of access to PA-5 and submit a revised fuel modification plan addressing PA-5.

**CONCEPTUAL SITE PLAN**

**Legal Description**

(Refer to Civil for additional information)  
 Parcel 1: Portion of Rancho La Sierra Yorba. Recorded in book 637, page 432 of Deeds, Riverside County Records.  
 Parcel 2: Portion of Rancho La Sierra Yorba. Recorded in book 637, page 432 of Deeds, Riverside County Records.  
 Parcel 3: That portion of Tract "C" marked "A. B. No 5, in the County of Riverside, Recorded of the partition of Maria de Jesus Y De Scully Estates, filed Action No. 7939 of the Superior Court of The State of California, County of Riverside, Recorded in Book 637, page 432 of Deeds, Riverside County Records.

Source(s): Bastien and Associates, Inc. (03-09-2023)

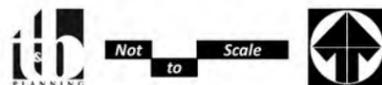


Figure 3-3

west, north, and east of the proposed buildings, with additional passenger vehicle parking proposed along the south side of Building 3. Access to Buildings 2 and 3 would be accommodated by driveways extending from Street A.

- Buildings 4 and 5 are proposed within proposed Planning Area 3. Building 4 would contain a total of 100,155 s.f. of building area (93,868 s.f. building footprint and 6,287 s.f. of mezzanine space). Building 5 would contain a total of 86,600 s.f. of building area (80,320 s.f. building footprint and 6,280 s.f. of mezzanine space). As shown on Figure 3-2, truck trailer loading docks are proposed along the east side of Building 4 and along the west side of Building 5. Passenger vehicle parking areas are proposed to the west, south, and east of the proposed buildings, with additional passenger vehicle parking proposed along the north side of Building 5. Access to Buildings 4 and 5 would be accommodated by two driveways extending from Street A, and a single driveway extending from Dominguez Ranch Road.

### 3.4.2 PP2020-0004 Proposed Architecture

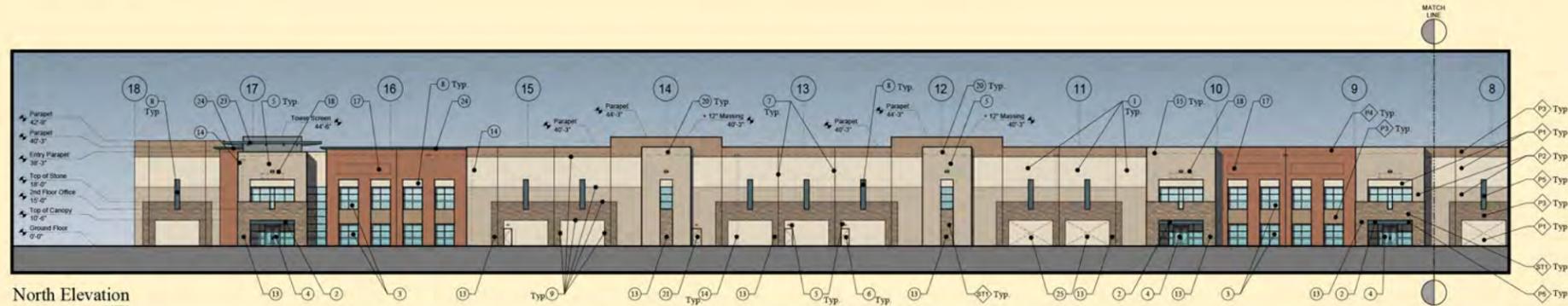
PP2020-004 also includes plans for proposed architectural treatment of the five proposed buildings. Proposed architectural characteristics are described below for each building.

As shown on Figure 3-4 and Figure 3-5, *Building 1 Conceptual Elevations*, Building 1 is proposed with concrete tilt-up panels. Building 1 would have a variable roofline with screening elements extending to a height of 46' 6", parapets extending to a height of 44' 3", and remaining portions of the building façade extending to a height of 40' 3". Areas proposed for ancillary office uses along the northern side of the building would include glazing (glass) elements, and would be articulated with stone veneer. Building 1 is proposed to include a mix of colors, generally including brown and light brown colors near the proposed ancillary office uses along the north side of the building, and light brown and off-white colors along the remaining portions of the building. Metal mechanical screens are proposed above the proposed ancillary office spaces to shield roof-top mechanical equipment from public view. Decorative metal fencing also is proposed to screen the proposed truck docking area from public view.

As shown on Figure 3-6, *Building 2 Conceptual Elevations*, and Figure 3-7, *Building 3 Conceptual Elevations*, the architectural treatments proposed for Buildings 2 and 3 are similar to those described above for Building 1. Both buildings are proposed with concrete tilt-up panels. Building 2 would have a variable roofline, with screening elements extending to a height of 43' 0", parapets extending to a height of 40' 9" and remaining portions of the building façade extending to a height of 36' 9". Building 3 would have a variable roofline, with screening elements extending to a height of 43' 0", parapets extending to a height of 38' 9" and remaining portions of the building façade extending to a height of 37' 9". The northwest and southwest corners of Building 2 and the northeast and southeast corners of Building 3 are proposed to include ancillary office space, and would be treated with stone veneer and glazing (glass) elements. Both buildings are proposed to include a mix of colors, generally including brown and light brown colors near the proposed ancillary office uses along the western side of Building 2 and the eastern side of Building 3, and light brown and off-white colors along the remaining portions of the buildings. Metal mechanical screens are proposed above the proposed ancillary office spaces to shield roof-top mechanical equipment from public view. Decorative metal fencing also is proposed to screen the proposed truck docking area located between the two buildings from public view.



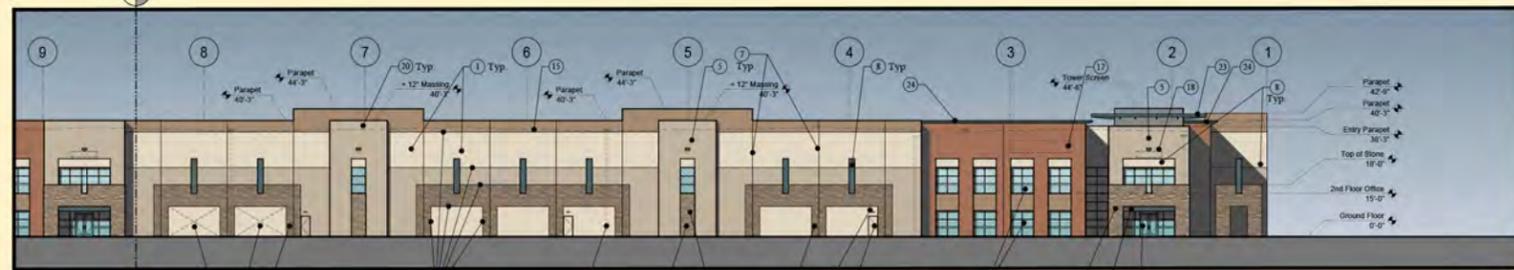
Overall North Elevation (Not to Scale)



North Elevation

Finish Legend

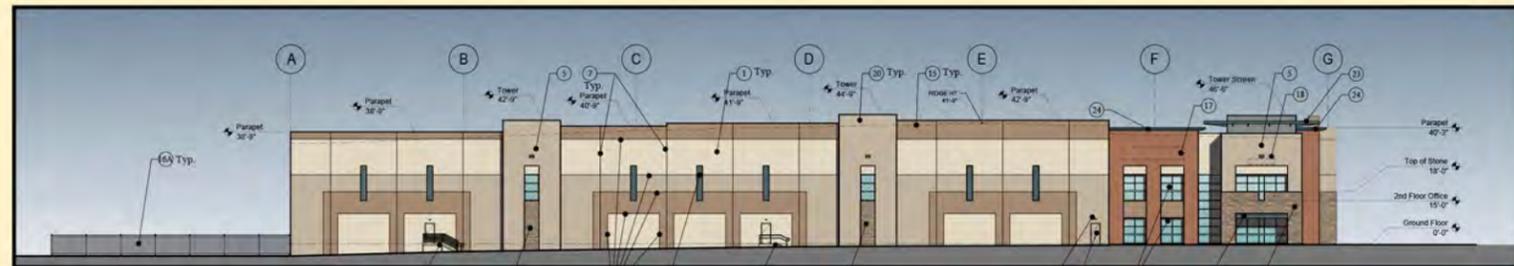
	PAINT - P-1 DUNN EDWARDS EXCLUSIVE PORY DE4191		PAINT - P-2 DUNN EDWARDS PIGION GRAY DE4134
	PAINT - P-3 DUNN EDWARDS WINDS WREATH DE4134		PAINT - P-4 DUNN EDWARDS CIBC BRICK DE4134
	PAINT - P-5 (Accent/Canopy) DUNN EDWARDS IRON CREEK DE3775		STONE VENEER - ST-1 MFG. CREATIVE MENS STYLE: TSD COLOR: TSD



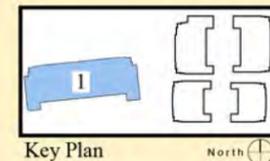
North Elevation (Continued)

Elevation Key Notes

- Concrete Tilt-up Panels with Joints and Reveals as Indicated. Painted - Typical
- Metal Clad Canopy - Best 090° Thick Aluminum Panel w/ Kawneer Finish (Clear Anodized) over Steel Frame - Typ
- Clear Anodized, Front Glazed, Aluminum Mullion System w/ Reflective Blue-Green Glass - Typ (Cross Hatch Areas Indicate Spandrel Conditions)
- 8'-0" High Medium Side Entry Doors - Typ
- Light Fixture per Photometric
- 3/4" Steel Man Door and Frame Painted to Match Adjacent Wall Surface Panel Joint - Typ
- 3/4" Reveal - Typ
- 3/4" Reveal - Typ
- Concrete Guardrail Walls With Painted Metal Handrails
- 9'x10' Steel Panel Sectional Door Painted to Match Adjacent Wall Surface (Located 48" Above Exterior Grade)
- 12'x14' Steel Panel Sectional Door Painted to Match Adjacent Wall Surface
- Stone Veneer - Extent as Indicated
- Internal Down Spout with Overflow Scupper Painted to Match Adjacent Wall - Typical
- Dashed Line Represents Roof Line at Backside of Parapet
- Decorative Metal Fencing (All new Gates to have Corresponding Knock Box as Required by Fire Dept.)
- 9'-0" High, Decorative Metal Fence (Picket Style) (Painted Black - Typical)
- 9'-0" High, Decorative Rolling Metal Gate (Picket Style) (Painted Black - Typical)
- Proposed Building/Tenant Identification Signage Location
- Proposed Address Location (16" Tall Letters)
- ESFR Pump House (Concrete Tilt-Up, Painted) (Building 1 only)
- Painted Concrete Tilt-Up, Architectural Pop-Out Design Element (Per Plans)
- Electric Room Exterior Access Door, Per Plans
- Not Used
- Decorative Louver - Metal Clad, Best 090° Thick Aluminum Panel w/ Kawneer Finish over Steel Frame - Typ
- Decorative Cornice - Metal Clad, Best 090° Thick Aluminum Panel w/ Kawneer Finish over Steel Frame - Typ
- Knock-Out Panel (4 Total - Bldg. 1 Only)

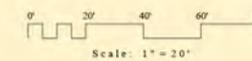


East Elevation



Key Plan

BUILDING #1 CONCEPTUAL ELEVATIONS



Scale: 1" = 20'

Source(s): Bastien and Associates, Inc. (07-16-2021)

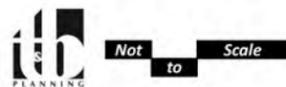


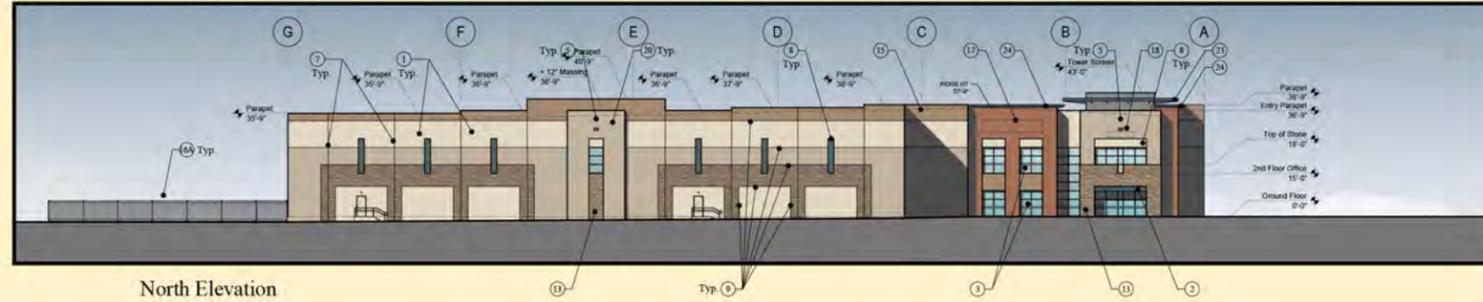
Figure 3-4



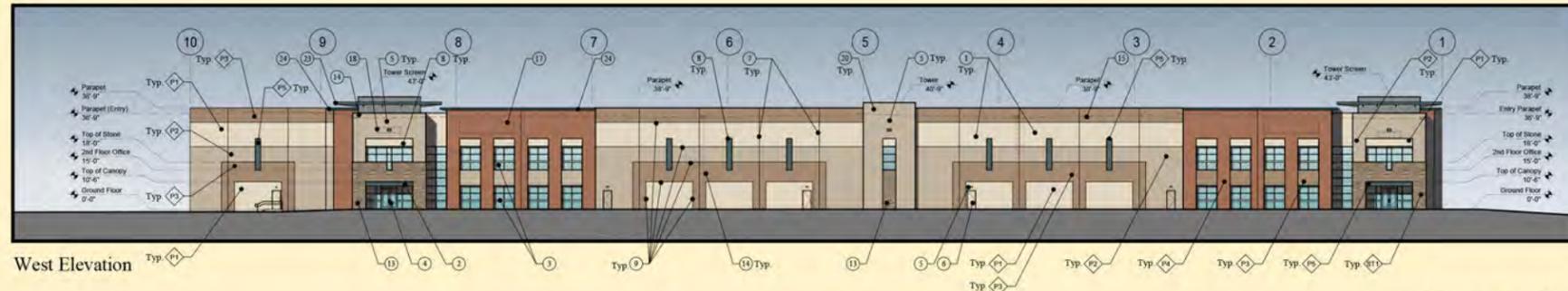
Source(s): Bastien and Associates, Inc. (07-16-2021)

Figure 3-5

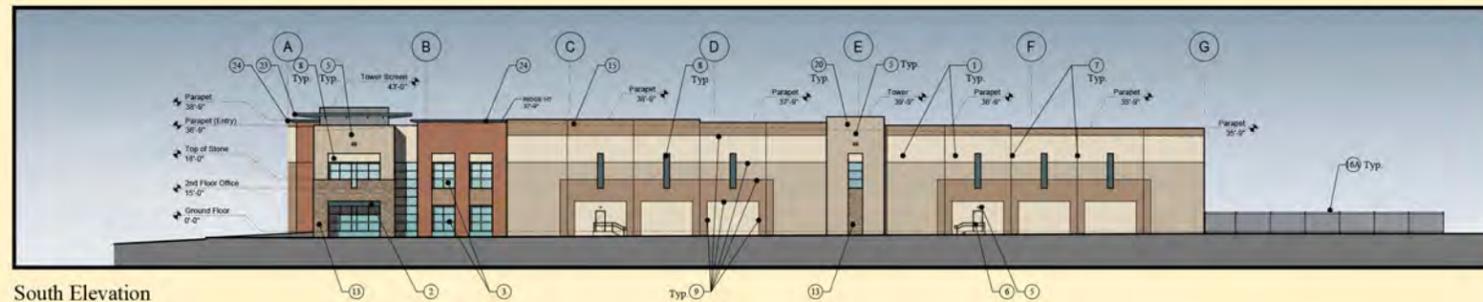




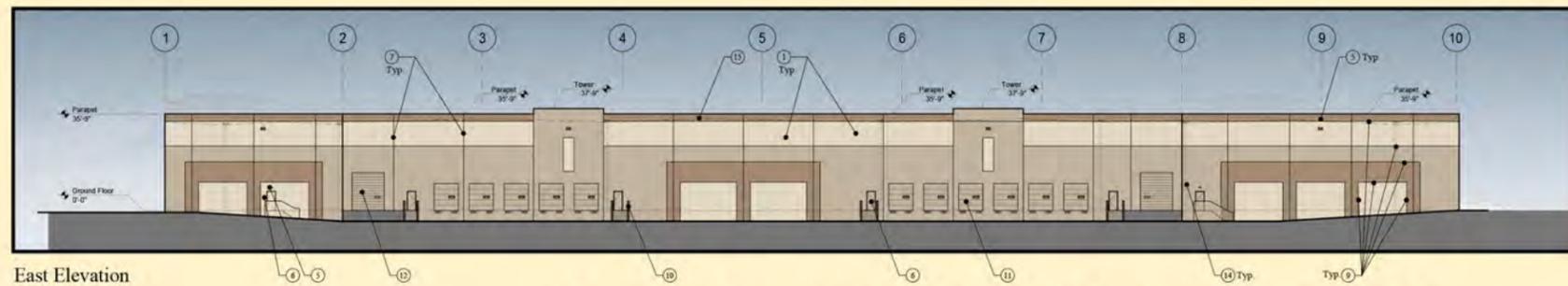
North Elevation



West Elevation



South Elevation



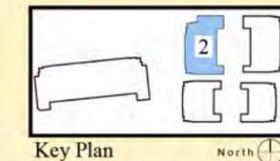
East Elevation

Finish Legend

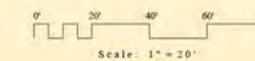
	PAINT - P-1 DUNN EDWARDS EXCLUSIVE PIVORY DE6154		PAINT - P-2 DUNN EDWARDS PIGION GRAY DE6114
	PAINT - P-3 DUNN EDWARDS WHOLE WHEAT DE6134		PAINT - P-4 DUNN EDWARDS CHOC BRICK DE6104
	PAINT - P-5 (Accent/Canopy) DUNN EDWARDS IRON CREEK DE3775		STONE VENEER - ST-1 MFG. CREATIVE MONES STYLE: TSD COLOR: TBD

Elevation Key Notes

- 1 Concrete Tilt-up Panels with Joints and Reveals as Indicated. Painted - Typical
- 2 Metal Clad Canopy - Best 090° Thick Aluminum Panel w/ Kawneer Finish (Clear Anodized) over Steel Frame - Typ
- 3 Clear Anodized, Front Glazed, Aluminum Mullion System w/ Reflective Blue-Green Glass - Typ (Cross Hatch Areas Indicate Spandrel Conditions)
- 4 8'-0" High Medium Side Entry Doors - Typ
- 5 Light Fixture per Photometric
- 6 1/2" Steel Man Door and Frame Painted to Match Adjacent Wall Surface Panel Joint - Typ
- 7 3/4" Recess - Typ
- 8 3/4" Reveal - Typ
- 9 Concrete Guardrail Walls With Painted Metal Handrails
- 10 9'x10' Steel Panel Sectional Door Painted to Match Adjacent Wall Surface (Located 48" Above Exterior Grade)
- 11 12'x14' Steel Panel Sectional Door Painted to Match Adjacent Wall Surface
- 12 Stone Veneer - Extent as Indicated
- 13 Internal Down Spout with Overflow Scupper Painted to Match Adjacent Wall - Typical
- 14 Dashed Line Represents Roof Line at Backside of Parapet
- 15 Decorative Metal Fencing (All new Gates to have Corresponding Knock Box as Required by Fire Dept.)
- 16 9'-0" High, Decorative Metal Fence (Picket Style) (Painted Black - Typical)
- 17 9'-0" High, Decorative Rolling Metal Gate (Picket Style) (Painted Black - Typical)
- 18 Proposed Building/Tenant Identification Signage Location
- 19 Proposed Address Location (16" Tall Letters)
- 20 ESRF Pump House (Concrete Tilt-Up, Painted) (Building 1 only)
- 21 Painted Concrete Tilt-Up, Architectural Pop-Out Design Element (Per Plans)
- 22 Electric Room Exterior Access Door, Per Plans
- 23 Not Used
- 24 Decorative Louver - Metal Clad - Best 090° Thick Aluminum Panel w/ Kawneer Finish over Steel Frame - Typ
- 25 Decorative Cornice - Metal Clad - Best 090° Thick Aluminum Panel w/ Kawneer Finish over Steel Frame - Typ
- 26 Knock-Out Panel (4 Total - Bldg. 1 Only)



Key Plan



BUILDING #2 CONCEPTUAL ELEVATIONS

Source(s): Bastien and Associates, Inc. (07-16-2021)

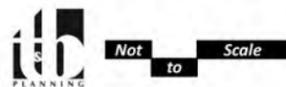
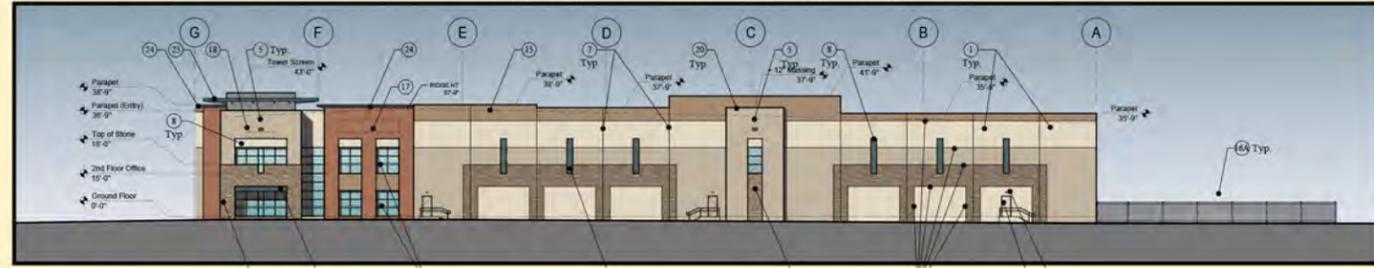


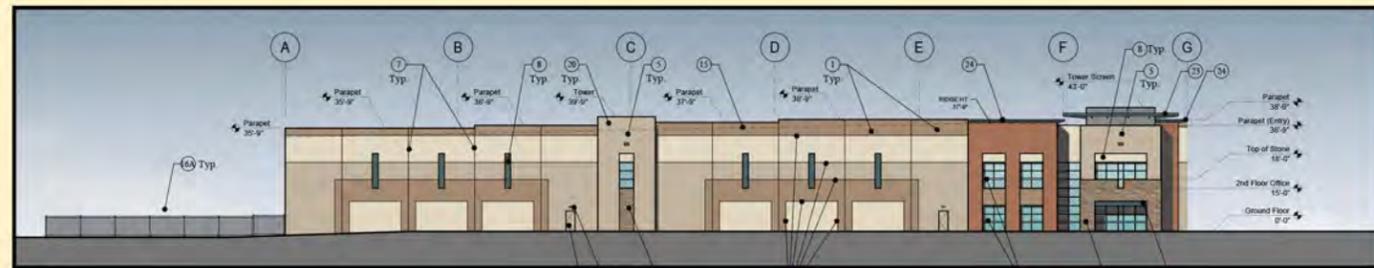
Figure 3-6



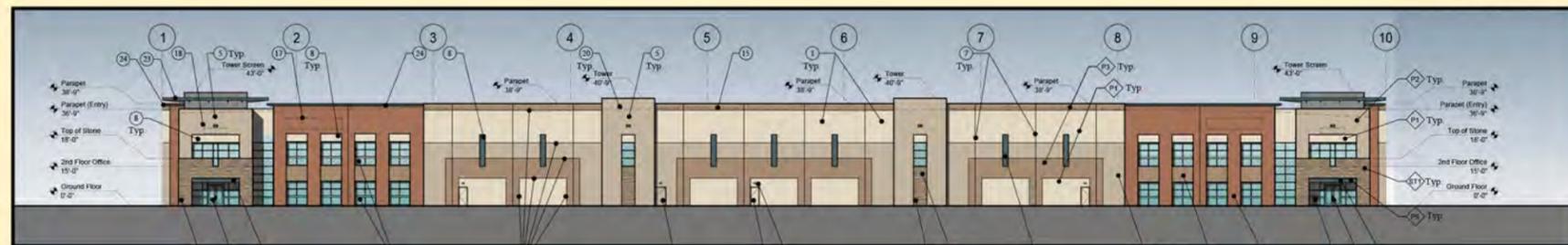
North Elevation



West Elevation



South Elevation



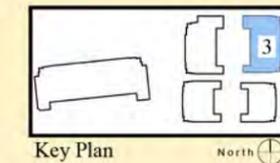
East Elevation

Finish Legend

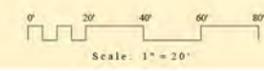
	PAINT - P-1 DUNN EDWARDS EXCLUSIVE PORY DE414		PAINT - P-2 DUNN EDWARDS PIGION GRAY DE414
	PAINT - P-3 DUNN EDWARDS WHOLE WHEAT DE414		PAINT - P-4 DUNN EDWARDS CHOC BRICK DE414
	PAINT - P-5 (Accent/Canopy) DUNN EDWARDS IRON CREEK DE3775		STONE VENEER - ST-1 MFG. CREATIVE MENS STYLE: TSD COLOR: TSD

Elevation Key Notes

- 1 Concrete Tilt-up Panels with Joints and Reveals as Indicated. Painted - Typical
- 2 Metal Clad Canopy. Best 090° Thick Aluminum Panel w/ Kawneer Finish (Clear Anodized) over Steel Frame - Typ
- 3 Clear Anodized, Front Glazed, Aluminum Mullion System w/ Reflective Blue Green Glass - Typ (Cross Hatch Areas Indicate Spandrel Conditions)
- 4 8'-0" High Medium Side Entry Doors - Typ
- 5 Light Fixture per Photometric
- 6 1/2" Steel Man Door and Frame Painted to Match Adjacent Wall Surface. Panel Joint - Typ
- 7 3/4" Recess - Typ
- 8 3/4" Reveal - Typ
- 9 Concrete Guardrail Walls With Painted Metal Handrails
- 10 9'x10' Steel Panel Sectional Door Painted to Match Adjacent Wall Surface (Located 48" Above Exterior Grade)
- 11 12'x14' Steel Panel Sectional Door Painted to Match Adjacent Wall Surface
- 12 Stone Veneer - Extent as Indicated
- 13 Internal Down Spout with Overflow Scupper Painted to Match Adjacent Wall - Typical
- 14 Dashed Line Represents Roof Line at Backside of Parapet
- 15 Decorative Metal Fencing (All new Gates to have Corresponding Knock Box as Required by Fire Dept.)
- 16 9'-0" High, Decorative Metal Fence (Picket Style) (Painted Black - Typical)
- 17 9'-0" High, Decorative Rolling Metal Gate (Picket Style) (Painted Black - Typical)
- 18 Proposed Building/Tenant Identification Signage Location
- 19 Proposed Address Location (16" Tall Letters)
- 20 ESRF Pump House (Concrete Tilt-Up, Painted) (Building 1 only)
- 21 Painted Concrete Tilt-Up, Architectural Pop-Out Design Element (Per Plans)
- 22 Electric Room Exterior Access Door, Per Plans
- 23 Not Used
- 24 Decorative Louver - Metal Clad. Best 090° Thick Aluminum Panel w/ Kawneer Finish over Steel Frame - Typ
- 25 Decorative Cornice - Metal Clad. Best 090° Thick Aluminum Panel w/ Kawneer Finish over Steel Frame - Typ
- 26 Knock-Out Panel (4 Total - Bldg. 1 Only)



Key Plan



BUILDING #3 CONCEPTUAL ELEVATIONS

Source(s): Bastien and Associates, Inc. (07-16-2021)

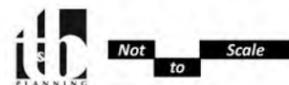


Figure 3-7

As shown on 0, *Building 4 Conceptual Elevations*, and Figure 3-8, *Building 5 Conceptual Elevations*, the architectural treatments proposed for Buildings 4 and 5 are similar to those described above for Buildings 1, 2, and 3. Both buildings are proposed with concrete tilt-up panels. Building 4 would have a variable roofline, with screening elements extending to a height of 43' 0", parapets extending to a height of 39' 9" and remaining portions of the building façade extending to a height of 35' 9". Building 5 also would have a variable roofline, with screening elements extending to a height of 43' 0", parapets extending to a height of 40' 9" and remaining portions of the building façade extending to a height of 38' 9". The northwest and southwest corners of Building 4 and the northeast and southeast corners of Building 5 are proposed to include ancillary office space, and would be treated with stone veneer and glazing (glass) elements. Both buildings are proposed to include a mix of colors, generally including brown and light brown colors near the proposed ancillary office uses along the western side of Building 4 and the eastern side of Building 5, and light brown and off-white colors along the remaining portions of the buildings. Metal mechanical screens are proposed above the proposed ancillary office spaces to shield roof-top mechanical equipment from public view. Decorative metal fencing also is proposed to screen the proposed truck docking area located between the two buildings from public view.

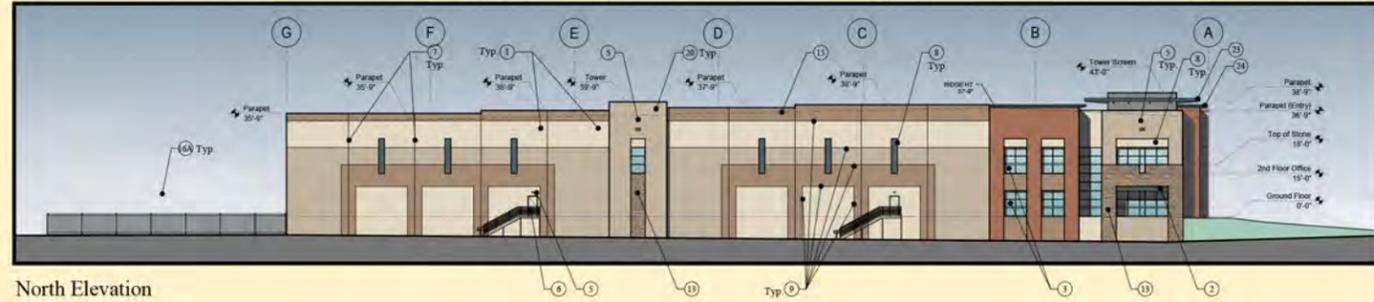
### 3.4.3 PP2020-0004 Landscaping Plan

A conceptual landscape plan also is included as part of PP2020-0004, and is depicted on Figure 3-9, *Conceptual Landscape Plan*. As shown, the areas proposed for "Business Park Industrial" land uses would be landscaped with a combination of trees, shrubs, and groundcover. As shown, the frontages along Green River Road, Fresno Road, and Dominguez Ranch Road would accommodate a landscaping buffer along proposed manufactured slopes measuring up to 170 feet in width. The parkways of these roads along the site's frontage would include 24-inch box coast live oak trees (*Quercus agrifolia*), while trees within the landscape buffer would include 24-inch box California sycamore (*Platanus racemosa*), 24-inch box California laurel (*Umbellularia californica*), and 24-inch box crape myrtle (*Lagerstroemia indica*).

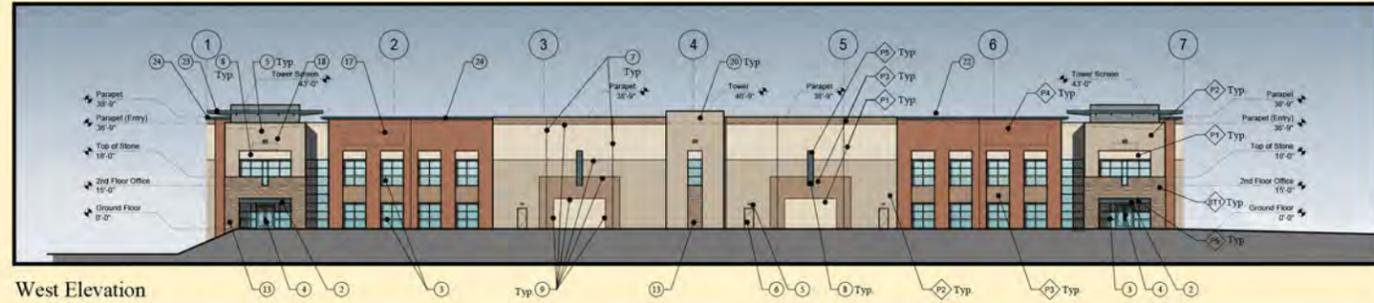
Enhanced landscaping treatments are proposed at the southern corner of the intersection of Green River Road and Fresno Road, at the main entrance to the site from Green River Road via proposed Street A, and at the southwest corner of Green River Road and Dominguez Ranch Road. At the main entrance along Street A, the street corners would include a major entry sign, 48-inch box coast live oak trees, 24-inch box forest pansy redbud (*Cercis canadensis* 'Forest Pansy') accent trees, and shrubs including creeping sage (*Salvia sonomensis*), pink muhly (*Muhlenbergia capillaris*), artichoke agave (*Agave parryi*), and century plant (*Agave americana*). Enhanced landscaping elements at the intersections of Fresno Road at Green River Road and Dominguez Ranch Road at Green River Road would be similar, but would feature a minor (i.e., smaller) entry sign and corner enhancement landscaping.

Internal to the site, proposed parking areas and areas surrounding the proposed buildings would include a mixture of trees and groundcover. Trees species would include 24-inch box Chinese pistache (*Pistacia chinensis*), 24-inch box desert museum palo verdi (*Parkinsonia* 'Desert Museum'), 24-inch box coast live oak, and 24-inch box forest pansy redbud.

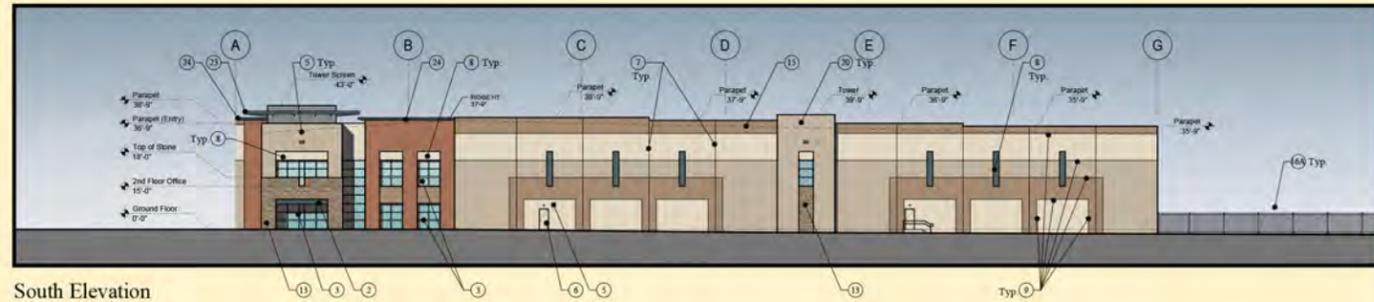
Landscaping also is proposed along the proposed manufactured slopes in the southern portions of the PP2020-0004 site, both for slope stabilization and aesthetic purposes. The slopes would be hydroseeded, and also would be landscaped with tree species including 24-inch box California laurel, 24-inch box coast live oak, and 24-inch box Catalina cherry (*Prunus lyoniana*).



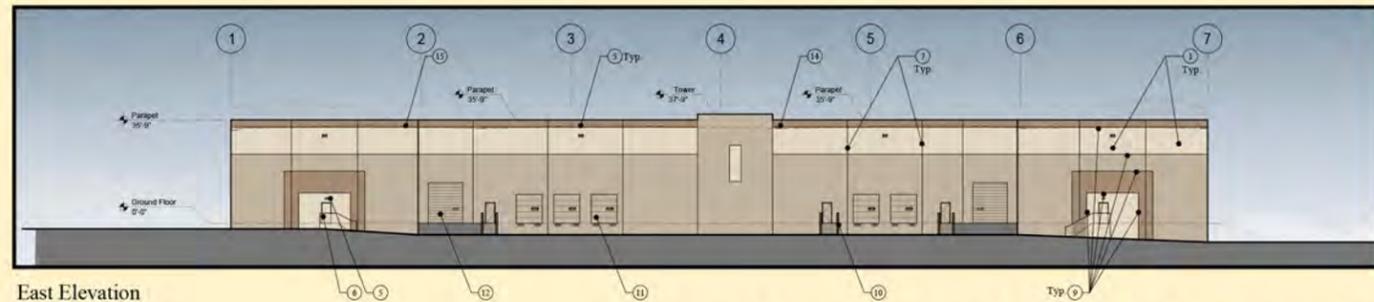
North Elevation



West Elevation



South Elevation



East Elevation

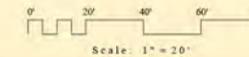
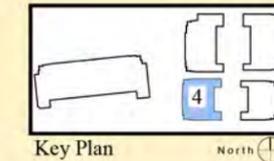
BUILDING #4 CONCEPTUAL ELEVATIONS

Finish Legend

	PAINT - P.1 DUNN EDWARDS EXCLUSIVE FORTY DE4151		PAINT - P.2 DUNN EDWARDS PIGION GRAY DE4114
	PAINT - P.3 DUNN EDWARDS WHOLE WHEAT DE4134		PAINT - P.4 DUNN EDWARDS CIBC BECK DE4104
	PAINT - P.5 (Accent/Canopy) DUNN EDWARDS IRON CREEK DE3775		STONE VENEER - ST-1 MFG. CREATIVE MONES STYLE: TSD COLOR: TSD

Elevation Key Notes

- 1 Concrete Tilt-up Panels with Joints and Reveals as Indicated. Painted - Typical
- 2 Metal Clad Canopy - Best 090° Thick Aluminum Panel w/ Kawneer Finish (Clear Anodized) over Steel Frame - Typ
- 3 Clear Anodized, Front Glazed, Aluminum Mullion System w/ Reflective Blue Green Glass - Typ (Cross Hatch Areas Indicate Spandrel Conditions)
- 4 8'-0" High Medium Side Entry Doors - Typ
- 5 Light Fixture per Photometric
- 6 3x7' Steel Man Door and Frame Painted to Match Adjacent Wall Surface
- 7 Panel Joint - Typ
- 8 3/4" Recess - Typ
- 9 3/4" Reveal - Typ
- 10 Concrete Guardrail Walls With Painted Metal Handrails
- 11 9x10' Steel Panel Sectional Door Painted to Match Adjacent Wall Surface (Located 48" Above Exterior Grade)
- 12 12x14' Steel Panel Sectional Door Painted to Match Adjacent Wall Surface
- 13 Stone Veneer - Extent as Indicated
- 14 Internal Down Spout with Overflow Scupper Painted to Match Adjacent Wall - Typical
- 15 Dashed Line Represents Roof Line at Backside of Parapet
- 16 Decorative Metal Fencing (All new Gates to have Corresponding Knock Box as Required by Fire Dept.)
- 17 9'-0" High, Decorative Metal Fence (Picket Style) (Painted Black - Typical)
- 18 9'-0" High, Decorative Rolling Metal Gate (Picket Style) (Painted Black - Typical)
- 19 Proposed Building/Tenant Identification Signage Location
- 20 Proposed Address Location (16" Tall Letters)
- 21 ESR Pump House (Concrete Tilt-Up, Painted) (Building 1 only)
- 22 Painted Concrete Tilt-Up, Architectural Pop-Out Design Element (Per Plans)
- 23 Electric Room Exterior Access Door, Per Plans
- 24 Not Used
- 25 Decorative Louver - Metal Clad, Best 090° Thick Aluminum Panel w/ Kawneer Finish over Steel Frame - Typ
- 26 Decorative Cornice - Metal Clad, Best 090° Thick Aluminum Panel w/ Kawneer Finish over Steel Frame - Typ
- 27 Knock-Out Panel (4 Total - Bldg. 1 Only)



Source(s): Bastien and Associates, Inc. (07-16-2021)

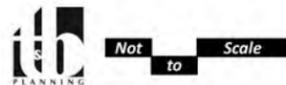
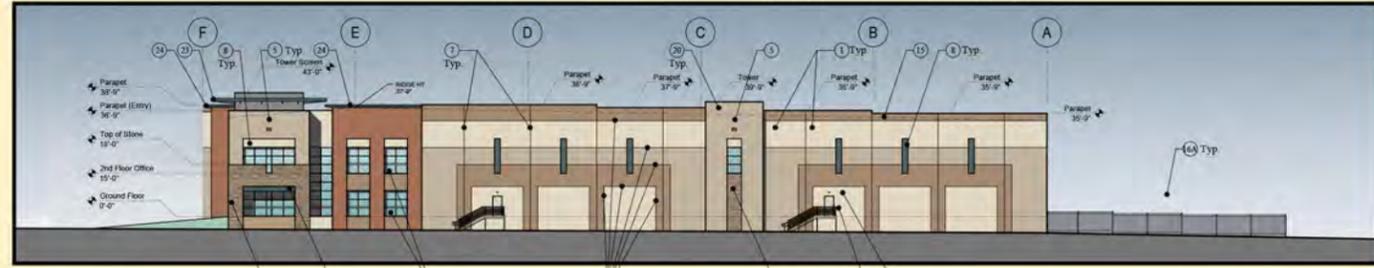
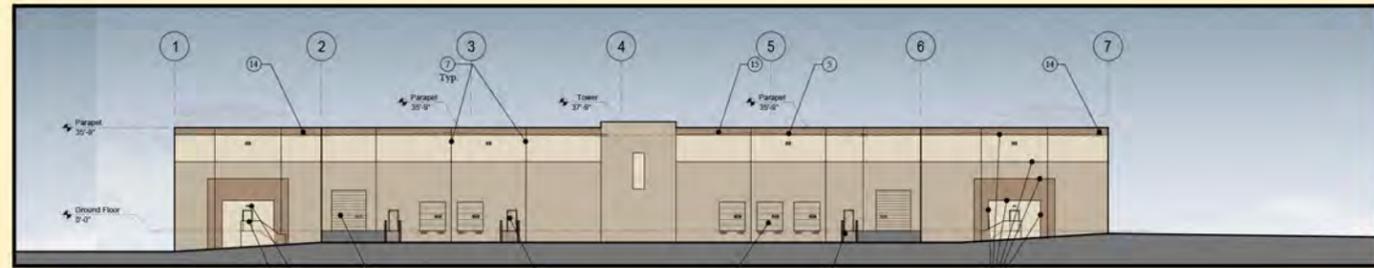


Figure 3-8

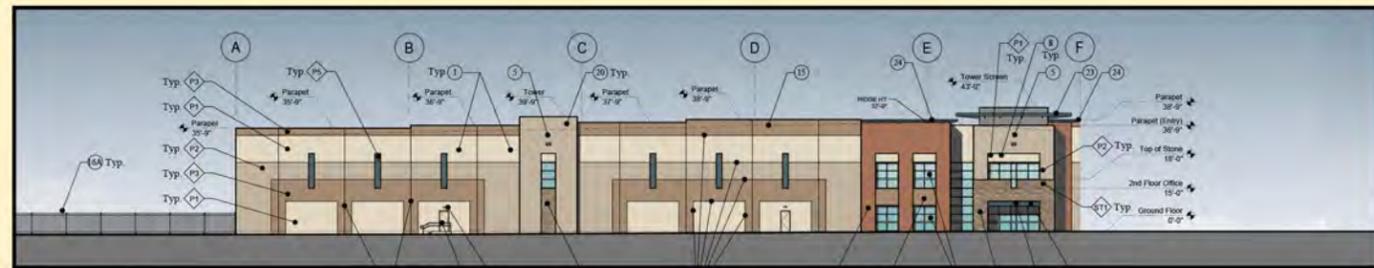
Building 4 Conceptual Elevations



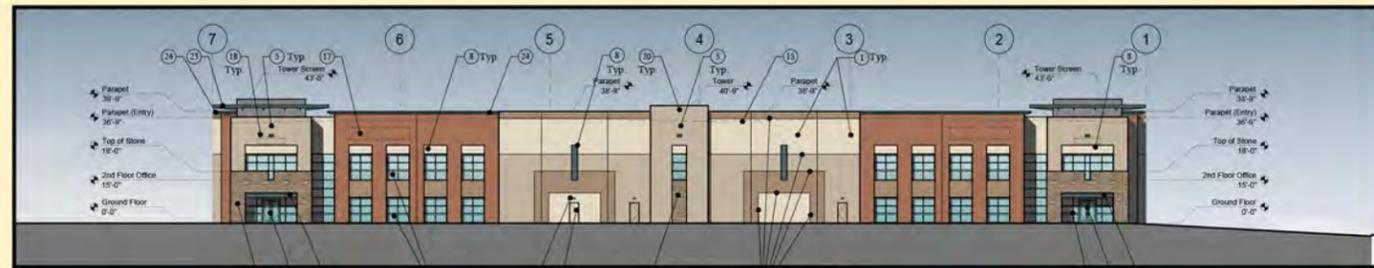
North Elevation



West Elevation



South Elevation



East Elevation

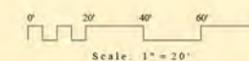
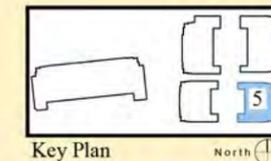
BUILDING #5 CONCEPTUAL ELEVATIONS

Finish Legend

	PAINT - P-1 DUNN EDWARDS EXCLUSIVE IVORY DE414		PAINT - P-2 DUNN EDWARDS PIGION GRAY DE414
	PAINT - P-3 DUNN EDWARDS WHOLE WHEAT DE414		PAINT - P-4 DUNN EDWARDS CHOC BRICK DE414
	PAINT - P-5 (Accent/Canopy) DUNN EDWARDS IRON CREEK DE3775		STONE VENEER - ST-1 MFG. CREATIVE MONES STYLE: TSD COLOR: TSD

Elevation Key Notes

- 1 Concrete Tilt-up Panels with Joints and Reveals as Indicated. Painted - Typical
- 2 Metal Clad Canopy - Best 090° Thick Aluminum Panel w/ Kawneer Finish (Clear Anodized) over Steel Frame - Typ
- 3 Clear Anodized, Front Glazed, Aluminum Mullion System w/ Reflective Blue Green Glass - Typ (Cross Hatch Areas Indicate Spandrel Conditions)
- 4 8'-0" High Medium Side Entry Doors - Typ
- 5 Light Fixture per Photometric
- 6 1/2" Steel Man Door and Frame Painted to Match Adjacent Wall Surface
- 7 Panel Joint - Typ
- 8 1/4" Recess - Typ
- 9 1/4" Reveal - Typ
- 10 Concrete Guardrail Walls With Painted Metal Handrails
- 11 9'x10' Steel Panel Sectional Door Painted to Match Adjacent Wall Surface (Located 48" Above Exterior Grade)
- 12 12'x14' Steel Panel Sectional Door Painted to Match Adjacent Wall Surface
- 13 Stone Veneer - Extent as Indicated
- 14 Internal Down Spout with Overflow Scupper Painted to Match Adjacent Wall - Typical
- 15 Dashed Line Represents Roof Line at Backside of Parapet
- 16 Decorative Metal Fencing (All new Gates to have Corresponding Knock Box as Required by Fire Dept.)
- 17 9'-0" High, Decorative Metal Fence (Picket Style) (Painted Black - Typical)
- 18 9'-0" High, Decorative Rolling Metal Gate (Picket Style) (Painted Black - Typical)
- 19 Proposed Building/Tenant Identification Signage Location
- 20 Proposed Address Location (16" Tall Letters)
- 21 ESR Pump House (Concrete Tilt-Up, Painted) (Building 1 only)
- 22 Painted Concrete Tilt-Up, Architectural Pop-Out Design Element (Per Plans)
- 23 Electric Room Exterior Access Door, Per Plans
- 24 Not Used
- 25 Decorative Louver - Metal Clad, Best 090° Thick Aluminum Panel w/ Kawneer Finish over Steel Frame - Typ
- 26 Decorative Cornice - Metal Clad, Best 090° Thick Aluminum Panel w/ Kawneer Finish over Steel Frame - Typ
- 27 Knock-Out Panel (4 Total - Bldg. 1 Only)



Source(s): Bastien and Associates, Inc. (07-16-2021)

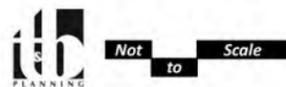
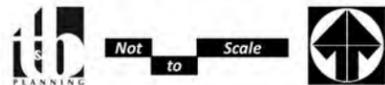


Figure 3-9



Source(s): Emerald Design (03-21-2023)

Figure 3-10



### 3.4.4 PP2020-0004 Conceptual Grading Plan

The grading plan for PP2020-0004 calls for grading of development pads to accommodate construction of the five (5) light industrial buildings in Planning Areas 1, 2, 3. The development pads are designed to be flat to very gently sloping to accommodate flat building foundations and parking areas for the buildings. The pads will be higher in elevation compared to Green River Road, and Street A will increase in grade as it extends south from Green River Road. Manufactured slopes are designed around the development pads, with the manufactured slopes transitioning into the natural condition on the south side of the Precise Plan's development envelope. The disturbed area is estimated at 61.13 acres, with approximately 1,267,300 cubic yards of cut and 1,120,300 cubic yards of fill. Earthwork volumes are expected to balance after compaction.

## 4.0 Evaluation Criteria

Based on the relevant CEQA Guidelines as implemented by the City of Corona, significant visual quality impacts would occur if the proposed Project or any Project-related component would:

- Have a substantial adverse effect on a scenic vista;
- Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway;
- In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings. (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the Project conflict with applicable zoning and other regulations governing scenic quality; or
- Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area.

## 5.0 Visual Impact Analysis

***Threshold a.: Would the Project have an adverse effect on a scenic vista?***

***Threshold c.: In non-urbanized areas, would the Project substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the Project is in an urbanized area, would the Project conflict with applicable zoning and other regulations governing scenic quality?***

A scenic vista is a view that possesses visual and aesthetic qualities of high value to the community. Scenic vistas can provide views of natural features or significant structures and buildings.

As previously depicted on Figure 2-3, under existing conditions the southern portions of the Project site are undeveloped and contain large, visually prominent undulating hillsides that are covered with natural vegetation primarily composed of grasslands and low-lying scrub. The northern portions of the property contain more gently sloping topography, with large areas used for horse stables and grazing, along with two existing single-family

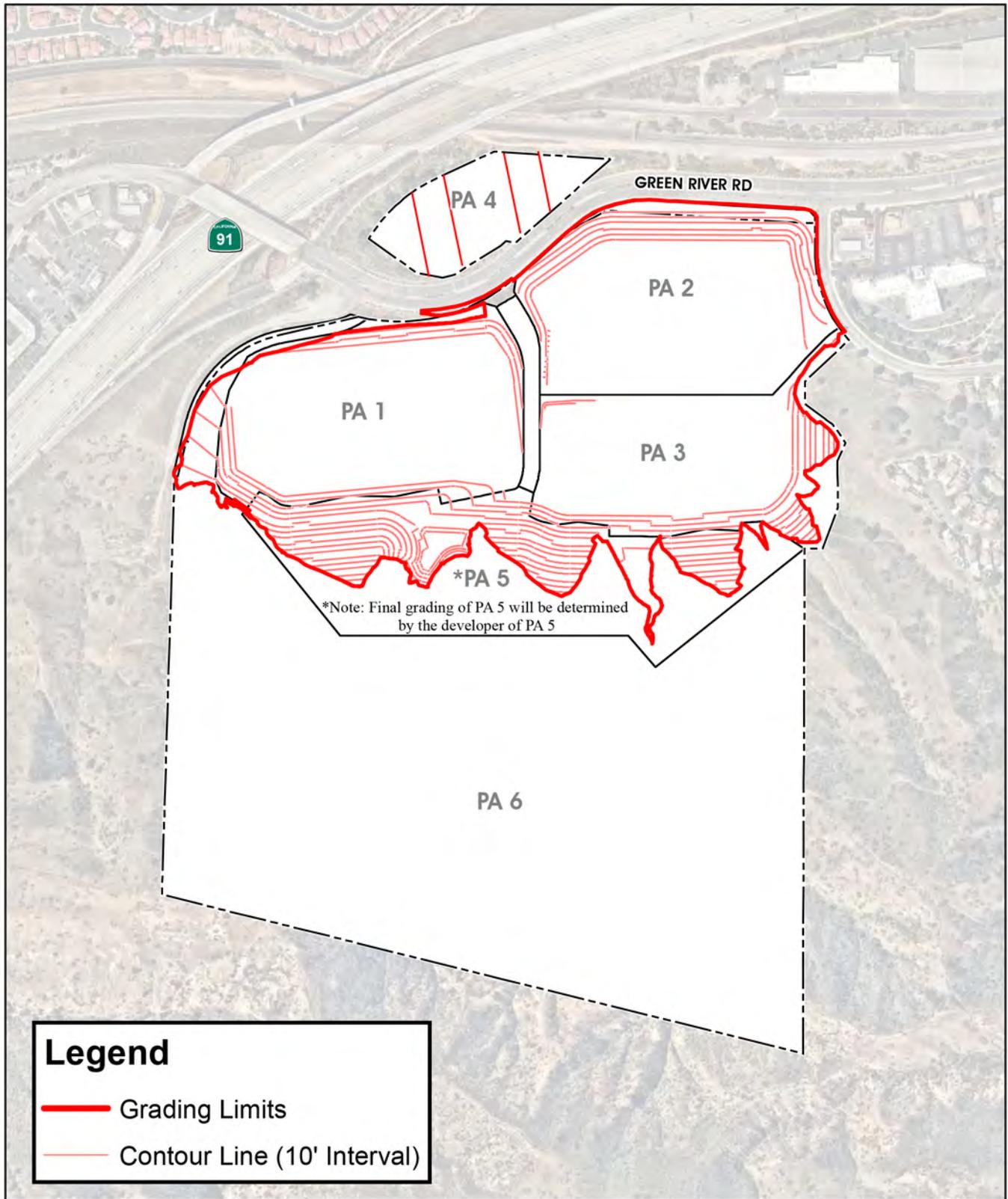


Figure 5-1

residences. Thus, under existing conditions, the northern portions of the property do not comprise a scenic vista as these areas are not highly visible from off-site locations, although the hillsides in the southern portions of the property are visually prominent and are a major component of the Project's viewshed.

With implementation of the Project, the northern portions of the property would be developed with "General Commercial" and "Business Park Industrial" land uses, while the southern portions of the site would be developed with up to 32 "Estate Residential" dwelling units. Development throughout the property would be required to comply with the development standards and design guidelines of the Green River Ranch Specific Plan, while development within the "Business Park Industrial" portions of the Project also would be required to comply with the site-specific components included as part of Precise Plan No. 2020-0004. The Green River Ranch Specific Plan and Precise Plan No. 2020-0004 implement the City of Corona General Plan policies related to aesthetics and comply with the City's Landscape Design Guidelines. Development in conformance with the Green River Ranch Specific Plan and Precise Plan No. 2020-0004 would ensure that the property is developed in a manner that is not aesthetically offensive.

The northern portions of the Project site that are proposed for "General Commercial" land uses occur at the property's lowest elevations and are not highly visible from off-site locations. This portion of the property is currently surrounded by the SR 91 freeway and exiting railroad tracks to the north and Green River Road to the south. Under existing conditions, there are two prominent billboards within this portion of the site. Additionally, lands to the east of this portion of the Project site are developed with business park and commercial retail uses, with residential and commercial uses occurring along the north side of SR 91. Development of the northern 5.5 acres of the Project site with "General Commercial" land uses would appear as a continuation of existing development patterns in the local area, and the proposed development would be visually compatible with the existing surrounding land uses. Additionally, due to the relatively low topography of this portion of the Project site as compared to surrounding areas, development of commercial retail uses as proposed would not obstruct any views of scenic resources, such as the existing hillforms in the southern portions of the site. Development of the proposed "General Commercial" land uses would not result in an adverse effect on any existing scenic vistas, and would not substantially degrade the existing visual character or quality of public views of the site and its surroundings; therefore, impacts would therefore be less than significant.

The Project's proposed "Business Park Industrial" land uses would occur in the northern portions of the Project site, within areas that largely contain gently sloping terrain. This portion of the Project site would be visible from areas to the north as well as from the existing residential homes to the east of the site. In order to evaluate the visual effects of the proposed Project, a series of renderings were prepared, which are depicted on Figure 5-2 and Figure 5-3 and are discussed below. It should be noted that the conceptual renderings were prepared only for the "Business Park Industrial" uses proposed as part of Precise Plan No. 2020-0004, and these renderings do not depict views of the proposed "General Commercial" uses in the northern portions of the site or the 32 "Estate Residential" uses proposed in the southern portions of the site.

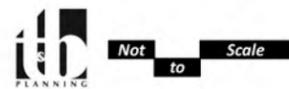
Figure 5-2, *Conceptual Rendering – Aerial Perspective*, provides a conceptual depiction of the proposed "Business Park Industrial" buildings proposed as part of Precise Plan No. 2020-0004. This conceptual rendering depicts views from an aerial perspective, looking south. As shown, the northern portions of the Project site would be graded to provide level pads for development. Landscape buffers are proposed along manufactured slopes to the north of



Overall View of Site From North of Hwy. 91 Facing South  
(200' Above Street Level)

Source(s): Bastien and Associates, Inc. (10-30-2020)

Figure 5-2



the proposed buildings. While some grading would occur at the base of the prominent hillforms on site, the proposed slopes would be contour graded to match to the extent possible the existing topography of these hillforms, and the manufactured slopes would be landscaped with hydroseed and trees. The large hillforms in the southern portions of the property would continue to be visible from off-site locations.

Figure 5-3, *Conceptual Rendering – Westerly Perspective*, depicts views from the existing single-family residential neighborhood to the east of the Project site, looking west. As shown from this perspective, the proposed “Business Park Industrial” buildings would be visible from this location. However, and as shown, development on site would not obstruct scenic vistas available from this location. The hillsides in the southern portion of the Project site would continue to be prominently visible from this location, and the proposed buildings would not obstruct distant views of the Chino Hills hillsides, which also are visible in the distance.

The conceptual renderings depicted on Figure 5-2 and Figure 5-3 demonstrate that the future development of the proposed “Business Park Industrial” buildings would not obstruct scenic vistas of the hillforms in the southern portions of the property, or distant views of the Chino Hills that are available from the existing single-family neighborhood to the east of the Project site, and would not substantially degrade the existing visual character or quality of public views of the site and its surroundings. Furthermore, and as previously noted, areas to the east and north of the Project site are developed with a mixture of commercial retail, business park, and residential land uses, and the proposed “Business Park Industrial” buildings would appear as a continuation of existing development patterns in the local area. As such, development of the proposed “Business Park Industrial” uses would not have an adverse effect on any existing scenic vistas in the area, and impacts would be less than significant.

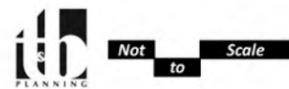
The southern portion of the Project site is proposed for development with up to 32 “Estate Residential” single-family dwelling units with a minimum lot size of 25,000 square feet, located in Planning Area 5 of the Green River Specific Plan. No specific development plans are proposed for this portion of the Project site at this time. Grading within this portion of the Project site would include the establishment of circulation access driveways and homesite pads for the proposed 32 dwelling units. Future grading and development would be required to comply with the Green River Ranch Specific Plan, which includes requirements to ensure that grading resembles the natural terrain as closely as possible. Additionally, landscaping along manufactured slopes would be required to be planted in an informal, more natural-looking arrangement consistent with the City’s Parks and Recreation Community Facilities District landscape design standards, to ensure sufficient plant coverage on the slopes, and to provide a transition between manufactured pads and natural hillsides. The proposed “Estate Residential” dwelling units and associated improvements would be visible from off-site locations; however, due to the low intensity of the proposed development and the design requirements of the Green River Ranch Specific Plan, the proposed homes would not substantially impact views of the existing on-site hillsides from off-site locations. Additionally, the proposed single-family homes would not obstruct or detract from views of other off-site scenic resources within the existing viewshed. Moreover, the proposed single-family homes would be visually compatible with and less intense than the existing residential development to the east of the Project site. Therefore, development of the proposed 32 single-family homes would not result in a significant, adverse effect on a scenic vista, and would not substantially degrade the existing visual character or quality of public views of the site and its surroundings; therefore, impacts would be less than significant.



View From San Viscaya Circle Facing West  
(Eye Level)

Source(s): Bastien and Associates, Inc. (07-16-2021)

Figure 5-3



***Threshold b.: Would the Project substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?***

As previously depicted on Figure 2-4, the City of Corona General Plan indicates that Palisades Drive and the segment of Green River Road between Palisades Drive and SR 91 are classified as “City Designated” scenic corridors, while SR 91 and SR 71 are identified as “State Eligible” scenic highways. Development proposed as part of the Project would be visible from these facilities. It should be noted that SR 91 and SR 71 are not officially designated as scenic highways. Notwithstanding, the analysis provided below includes an evaluation of potential visual effects to these highways.

As previously depicted on Figure 2-3, under existing conditions the Project site does contain not any visually prominent rock outcroppings, and there are no historic buildings on site. The property contains numerous trees scattered throughout the property, primarily in association with the existing single-family homes in the northeastern portions of the site, along the site’s frontage with Dominguez Ranch Road, and within the portion of the site located north of Green River Road.

With implementation of the proposed Project, the northern portions of the Project site would be developed with “General Commercial” and “Business Park Industrial” land uses, while the southern portions of the site would be developed with up to 32 “Estate Residential” dwelling units, and including 83.34 acres dedicated for “Open Space-General” land uses. Development on site would be required to comply with the development standards and design guidelines included in the Green River Ranch Specific Plan, while development within the proposed “Business Park Industrial” portions of the Project would be required to comply with the conceptual design plans included as part of Precise Plan No. 2020-0004. Development in accordance with the Green River Ranch Specific Plan and Precise Plan No. 2020-0004 would ensure that the Project site is developed in a manner that is visually compatible with existing surrounding developments to the east and north of the Project site.

As previously discussed, the development of “General Commercial” uses in the northern 5.5 acres of the Project site would not be prominently visible from off-site locations. This portion of the Project site contains relatively low elevations as compared to the surrounding area, and this portion of the site is completely surrounded by the SR 91 freeway and railroad tracks to the north, and Green River Road to the south. Additionally, development of commercial retail uses on this portion of the property would be visually compatible with existing business park and commercial retail developments located to the east of the site, and with existing commercial retail uses located north of the SR 91. Moreover, there are no scenic resources on this portion of the Project site, such as prominently visible trees, rock outcroppings, or historic buildings, and future commercial retail uses would be required to include extensive landscaping in conformance with the Green River Ranch Specific Plan requirements. As such, impacts to scenic highways with development of the “General Commercial” uses would be less than significant.

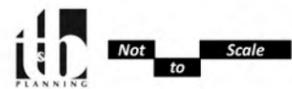
In order to evaluate the visual effects of the proposed “Business Park Industrial” buildings on nearby scenic highways, a series of conceptual renderings were prepared and are depicted on Figure 5-4 through Figure 5-7. The renderings depict views of the Project site along nearby segments of Green River Road, which is identified as a “City Designated” scenic corridor, and are representative of street-level views of the Project. As shown on these renderings, the proposed “Business Park Industrial” buildings would be developed on level development pads,



View From Green River Rd. at Fresno Rd. Facing Southeast  
(Street Level)

Source(s): Bastien and Associates, Inc. (07-16-2021)

Figure 5-4

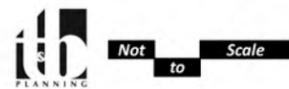




View From Green River Rd. West of Private Street "A" Facing Southeast  
(Street Level)

Source(s): Bastien and Associates, Inc. (07-16-2021)

Figure 5-5



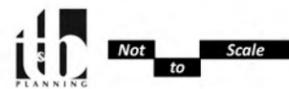
Street-Level Rendering – Green River Road West of Private Street “A”

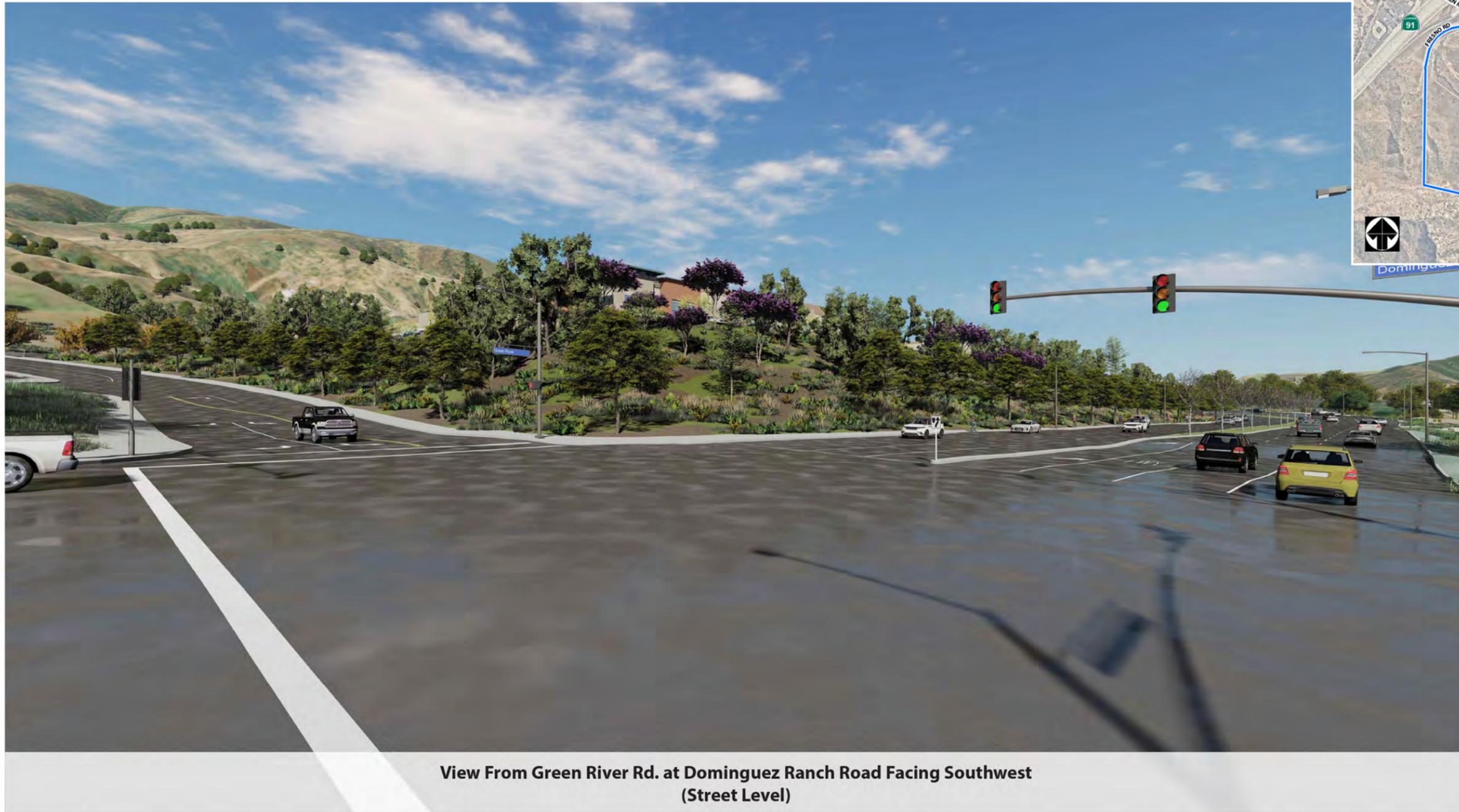


View From Green River Rd. Just East of Private Street "A" Facing Southwest  
(Street Level)

Source(s): Bastien and Associates, Inc. (07-16-2021)

Figure 5-6

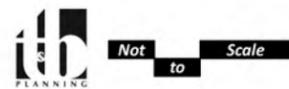




View From Green River Rd. at Dominguez Ranch Road Facing Southwest  
(Street Level)

Source(s): Bastien and Associates, Inc. (07-16-2021)

Figure 5-7



with manufactured slopes occurring along a majority of the Project site's frontage with Green River Road. The manufactured slopes would serve as a landscaped buffer between Green River Road and the proposed buildings, and would be heavily landscaped with trees, shrubs, and groundcover. Although development of the areas planned for "Business Park Industrial" land uses would result in a substantial change in the visual character of the northern portions of the property, areas to the north and east of the Project site already are developed with a mixture of commercial, business park, and residential uses, as well as major roadway facilities (i.e., SR 91) and railroad tracks. Development of the site as proposed would appear as a continuation of existing development patterns in the local area. Furthermore, under existing conditions the only potential scenic resources on this portion of the Project site are the scattered trees that are not visually prominent from off-site locations, and these trees would be replaced with ornamental tree species included as part of the Project's conceptual landscape plan. Therefore, development of the proposed "Business Park Industrial" buildings would not substantially affect scenic resources visible from nearby scenic highways, and impacts would therefore be less than significant.

Development of the 32 "Estate Residential" dwelling units would occur in the southern portions of the property, where there are no visually prominent trees. Additionally, the proposed 32 dwelling units would have a minimum lot size of 25,000 square feet, and would occur over approximately 20.39 acres of the property. Thus, while the proposed "Estate Residential" dwelling units would be visible from off-site locations, the general appearance of the southern portions of the Project site when viewed from off-site locations would continue to be dominated by the existing hillsides and natural vegetation. Thus, development of the 32 "Estate Residential" dwelling units would not substantially damage scenic resources visible from nearby "City Designated" scenic corridors or "State Eligible" scenic highways, and impacts would be less than significant.

Although the Project would result in a substantial change in the visual character of the property, mandatory compliance with the Green River Ranch Specific Plan development standards and design guidelines, as well as the site-specific development plans included as part of Precise Plan No. 2020-0004, would ensure that the Project does not substantially damage scenic resources visible from nearby scenic highways. Accordingly, impacts would be less than significant, and no mitigation measures would be required.

***Threshold d.: Would the Project create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?***

Under existing conditions, the Project site is largely vacant and undeveloped, with the exception of two existing single-family homes. Thus, the Project site under existing conditions contains few sources of artificial lighting.

Implementation of the proposed Project would result in the introduction of new lighting elements on site to illuminate parking areas, truck docking areas, commercial signage, and building entrances. Lighting elements primarily would be associated with the "General Commercial" and "Business Park Industrial" land uses proposed in the northern portions of the Project site within proposed Planning Areas 1 through 4. Lighting within the proposed "Estate Residential" uses in the southern portions of the site would be minimal and would be limited to landscape illumination and illumination on the proposed residential homes.

Lighting elements on site would be governed by applicable provisions of the City of Corona's Municipal Code. Specifically, Chapter 17.84 of the City's Municipal Code requires that "[a]ll areas of exterior lighting shall be designed to direct light downward with minimal spillover onto adjacent residences, sensitive land uses and open

space.” In addition, Chapter 17.76 of the City’s Municipal Code requires that “[a]ll outdoor lighting within parking areas shall be designed and arranged with the approval of the City Engineer to restrict to a minimum the effects of stray light on adjacent property and city streets.”

In order to demonstrate compliance with the City’s Municipal Code requirements, the Project’s Precise Plan application materials include photometric plans showing anticipated lighting levels within the “Business Park Industrial” portions of the Project, and are depicted on Figure 7-1, *Site Photometrics – Building 1*, and Figure 7-2, *Site Photometrics – Buildings 2, 3, 4 and 5*. The photometric plans demonstrate that proposed lighting associated with the “Business Park Industrial” buildings would not expose neighboring properties to excessive lighting levels and would not generate lighting levels that could adversely affect daytime or nighttime views in the local area. Photometric plans also would be required in the future prior to development within the “General Commercial” portions of the Project, which would be required to demonstrate that lighting levels would not adversely affect daytime or nighttime views in the local area. Accordingly, Project impacts due to the creation of new sources of substantial light that could adversely affect day or nighttime views in the area would be less than significant.

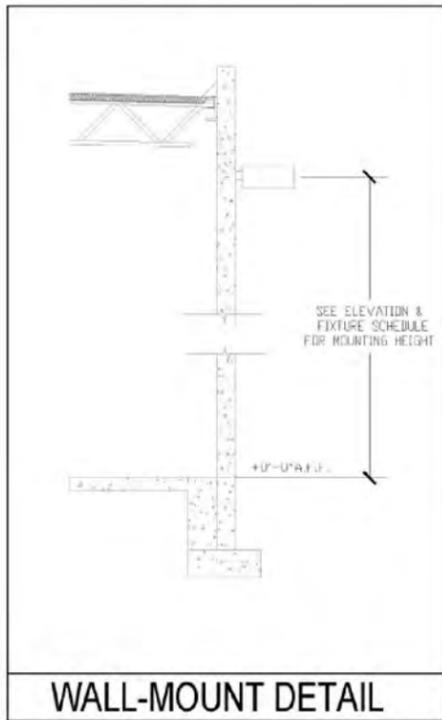
With respect to glare, a majority of the building elements proposed for the “Business Park Industrial” uses would consist of tilt-up concrete panels, although the portions of the buildings containing ancillary office uses would include glass elements. Additionally, proposed “General Commercial” buildings in the northern portion of the site also would include glass elements, as would the proposed “Estate Residential” uses. While window glazing has a potential to result in minor glare effects, such effects would not adversely affect daytime views of surrounding properties, including motorists along adjacent roadways, because the glass elements would be low-reflective. Areas proposed for window glazing also would be limited, as shown on the Project’s application materials. Furthermore, any potential glare effects would be reduced due to landscaping and perimeter walls and fencing. Thus, glare impacts from proposed building elements would be less than significant.

## **6.0 Mitigation Measures**

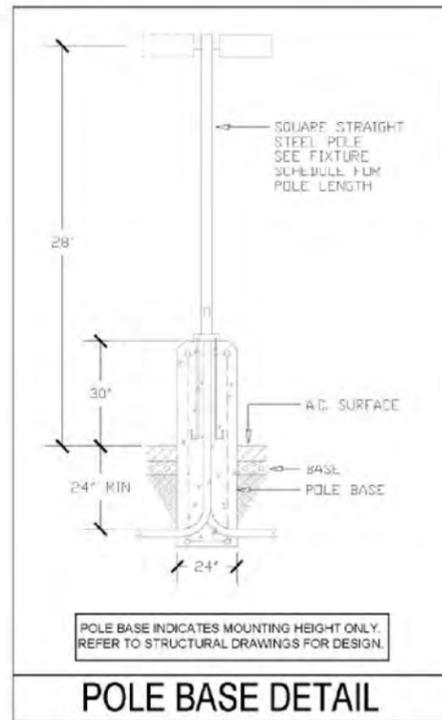
As indicated in the preceding analysis, with mandatory compliance with the development standards and design guidelines included as part of the Green River Ranch Specific Plan, as well as compliance with the site-specific development plans included as part of proposed Precise Plan No. 2020-0004, the proposed Project would result in less-than-significant visual quality impacts. As such, mitigation measures are not required.

## **7.0 Conclusion**

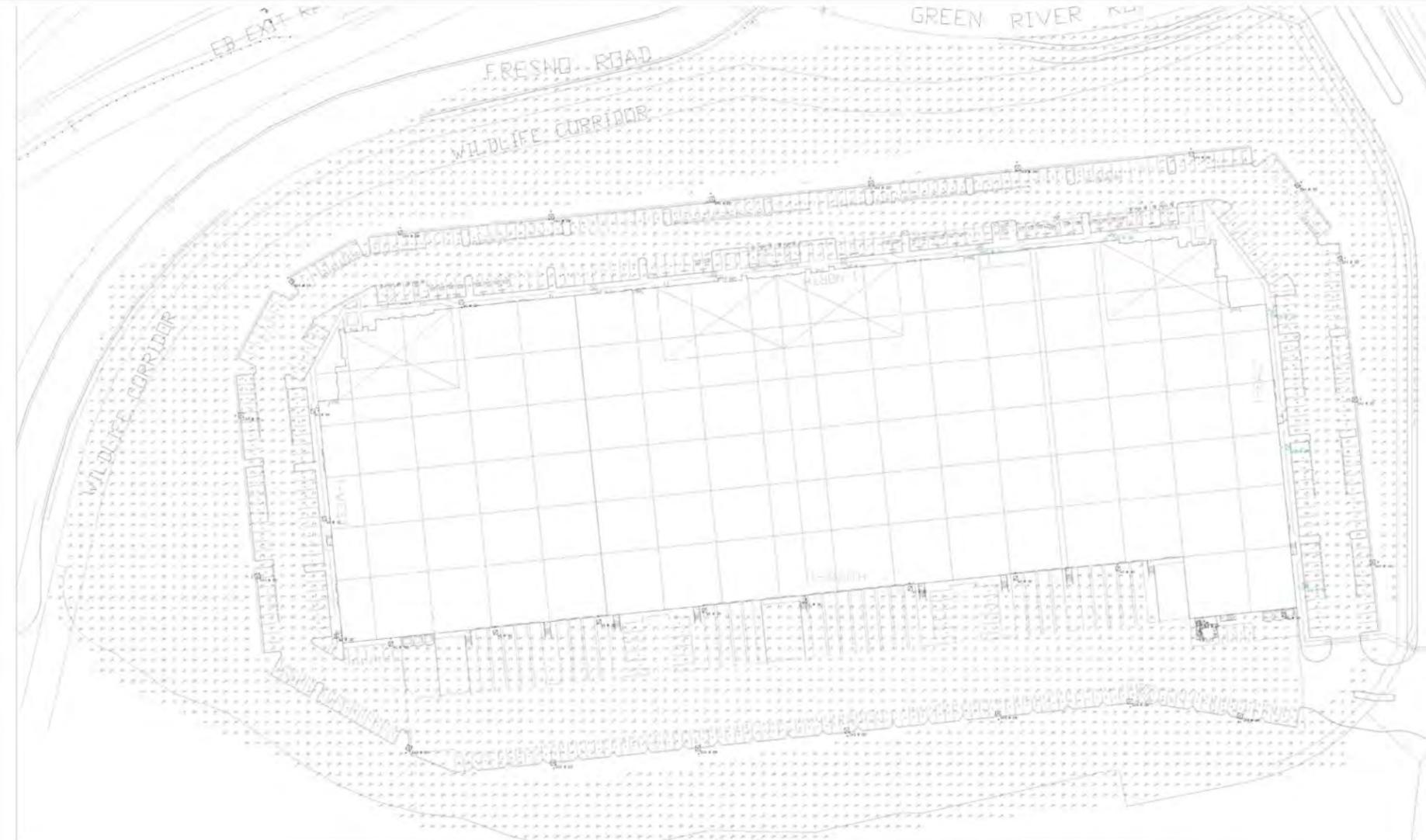
As indicated in the analysis presented in Section 5.0, with mandatory compliance with the development standards and design guidelines included as part of the Green River Ranch Specific Plan, as well as compliance with the site-specific development plans included as part of proposed Precise Plan No. 2020-0004, the proposed Project would not have a substantial adverse effect on a scenic vista; would not substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway; would not substantially degrade the existing visual character or quality of public views of the site and its surroundings; and would not create a new source of substantial light or glare which would adversely affect day or nighttime views in the area. Impacts would be less than significant, and mitigation measures are not required.



WALL-MOUNT DETAIL



POLE BASE DETAIL

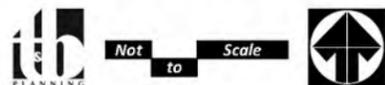


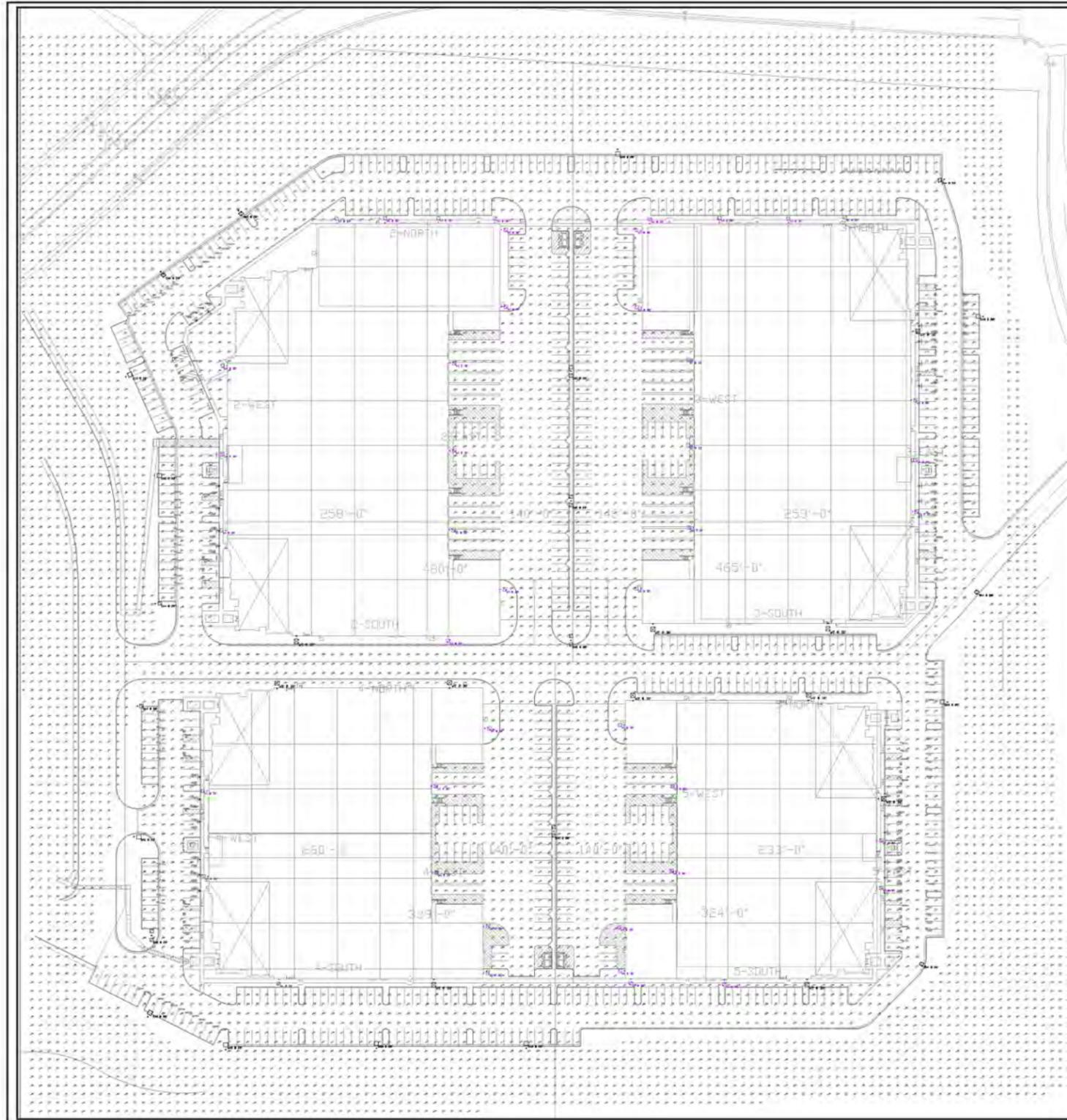
Symbol	Label	Image	Qty	Manufacturer	Catalog Number	Description	Loop	Number Lamps	Footcandle	Lumens per Lamp	Lumen Multiplier	LLF	Wattage
☑	W2		8	U.S. ARCHITECTURAL LIGHTING	VLL-PLD-III-W-BOLED-8011	BLACK PAINTED FINNISH METAL HOUSING WALL MT AT 30 FT AFG BUG 03 LD G3	80 WHITE LIGHT EMITTING DIODES (LEDs), BASE UP	80	VLL-PLD-III-V-208	1	0.9	1294	
☑	W1		14	U.S. ARCHITECTURAL LIGHTING	VLL-PLD-IV-BOLED-705NA-8011	BLACK PAINTED FINNISH METAL HOUSING WALL MT AT 30 FT AFG BUG 03 LD G3	80 WHITE LIGHT EMITTING DIODES (LEDs), BASE UP	60	VLL-PLD-IV-BOLED-705NA-295	1	0.9	1736	
☑	SA3		10	U.S. ARCHITECTURAL LIGHTING	VLL-PLD-III-W-BOLED-8011	BLACK PAINTED FINNISH METAL HOUSING WALL MT AT 28 FT AFG BUG RATING B1 LD G3	80 WHITE LIGHT EMITTING DIODES (LEDs), BASE UP	80	VLL-PLD-III-V-167	1	0.9	1294	
☑	SA4		5	U.S. ARCHITECTURAL LIGHTING	VLL-PLD-IV-FT-BOLED-8011	BLACK PAINTED FINNISH METAL HOUSING WALL MT AT 30 FT AFG BUG RATING B1 LD G3	80 WHITE LIGHT EMITTING DIODES (LEDs), BASE UP	80	VLL-PLD-IV-FT-166	1	0.9	1294	

Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Calc Zone #2		81 Fc/16	Fc/0.9	Fc	N/A	N/A
Calc Zone #3		88 Fc/16	Fc/0.9	Fc	5.01	8.21

Source(s): Bastien and Associates, Inc. (05-06-2021)

Figure 7-1

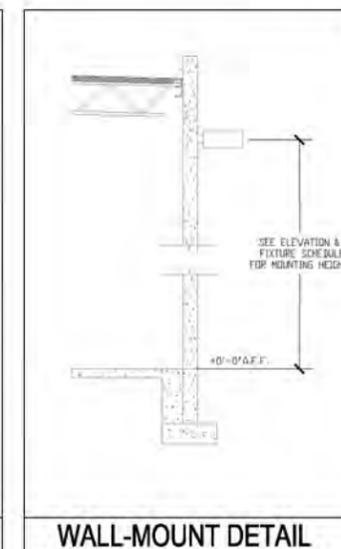
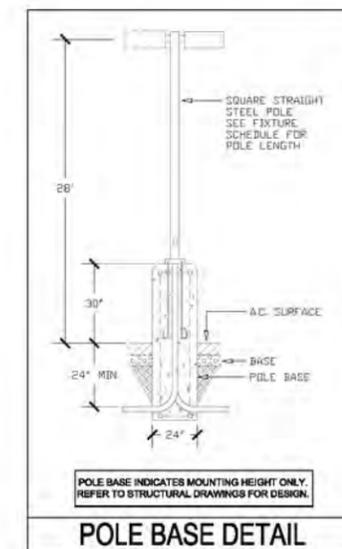




Luminaire Schedule									
Symbol	Label	QTY	Catalog Number	Description	Lamp	Number Lamps	Lumens per Lamp	LLF	Wattage
☒	SA2	5	VLL-PLD-VSQ-W-80LED-525mA-NW-MM511 POLE MT AT 28 FT AFG BUG RATING B5 U0 G3	CAST BLACK PAINTED FINNED METAL HOUSING.	80 WHITE LIGHT EMITTING DIODES (LEDS), BASE UP.	80	232.2484	0.9	258.8
☒	W1	44	VLL-PLD-IV-80LED-700mA-NW-MM511 BUG B3 U0 G3 WALL MT AT 30 FT AFG	CAST BLACK PAINTED FINNED METAL HOUSING.	80 WHITE LIGHT EMITTING DIODES (LEDS), BASE UP.	80	295.2534	0.9	173.6
☐	SA3	10	VLL-PLD-III-W-80LED-525mA-NW-HS POLE MT AT 28 FT AFG BUG RATING B1 U0 G3	CAST BLACK PAINTED FINNED METAL HOUSING.	80 WHITE LIGHT EMITTING DIODES (LEDS), BASE UP.	80	166.6557	0.9	129.4
☒	W2	11	VLL-PLD-III-W-80LED-1050mA-NW WALL MT AT 30 FT AFG	CAST BLACK PAINTED FINNED METAL HOUSING.	80 WHITE LIGHT EMITTING DIODES (LEDS), BASE UP.	80	367.1778	1	256.4
☒	SA4	7	VLL-PLD-IV-FT-80LED-525mA-MM511 NW-HS POLE MT AT 28 FT AFG BUG RATING B1 U0 G3	CAST BLACK PAINTED FINNED METAL HOUSING.	80 WHITE LIGHT EMITTING DIODES (LEDS), BASE UP.	80	166.3443	0.9	129.4

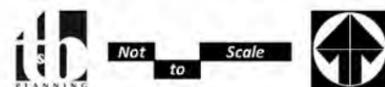
Statistics

Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Calc Zone #1	+	N/A	0.0 fc	N/A	N/A	N/A
Calc Zone #3	+	N/A	0.0 fc	N/A	N/A	N/A



Source(s): Bastien and Associates, Inc. (04-20-2021)

Figure 7-2



**This Page Intentionally Left Blank**